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Land at Rhyd-y-Car, Merthyr Tydfil  
(Merthyr Village):  
archaeological desk-based assessment

October 2000

A report for Capita Property Services Ltd  
by Martin Lawler BA MIFA



Contracts Division



*Cwm-du Drift (Cwm Glo),  
engine house and stables, looking northeast*

**GGAT report no. 2000/062  
Project no. A628**

**National Grid Reference: SO 041 056**

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## Summary

*A proposed development for housing, retail and leisure facilities, on the west side of Merthyr Tydfil, will affect an area of historic mining remains. An assessment has been made of the archaeological interests within the proposed development area.*

*The study has shown that most of the area was exploited intensively for coal and ironstone, particularly during the 19th century. A number of industrial archaeological sites have been identified, including mining sites, waste tips, watercourses, rail/tramroads, canal features and the remains of workers' housing. This industrial landscape was superimposed on an older setting of scattered farmsteads and fields, part of which also survives in the study area.*

*From the results of the assessment, the overall study area has been divided into eight Archaeological Areas of differing characteristics. Four areas of notable industrial archaeological interest have been identified: that of the Upper Cwm Glo on the west side of the study area (Area 1), the central northern area (Area 3), the canal and tramroad corridor on the east side (Area 6) and a ribbon of early hillside outcrop workings (Area 4). A fourth area on the Nant Cwm Glo and Llwyn-yr-Eos stream valleys retains a cluster of early farmsteads (Area 2), which have historical significance for their association with the early Dissenter communities. Within the remaining areas, various individual features are also of potential archaeological interest.*

## Acknowledgements

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## Copyright Notice

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## Abbreviations

The following abbreviations have been used in this report:

NGR:	National Grid Reference
PRN:	Primary Record Number
NPRN:	National Primary Record Number
RCAHM(W):	Royal Commission on Ancient and Historic Monuments of Wales
SMR:	Sites and Monuments Record

NGRs are given in alphanumeric format and are accurate to eight figures (10 metres) or six figures (100 metres).

## 1.0 PROJECT BACKGROUND

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- 1.1 Capita Property Services Ltd are proposing to develop an area (Merthyr Village) to the east and west of the A470 near Rhyd-y-Car, Merthyr Tydfil, for housing, business and leisure facilities.
- 1.2 The Glamorgan-Gwent Archaeological Trust (Contracts Division) has been commissioned to carry out a preliminary desk-based assessment of the archaeological interests in the proposed development area.
- 1.3 Part of this area has been the subject of a previous application for reclamation work by Celtic Energy Ltd. An archaeological assessment of the latter area was submitted as part of the Environmental Impact Assessment for the scheme.<sup>1</sup>
- 1.4 The present report contains the results of the assessment of the study area.

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<sup>1</sup> Roberts, R 1997 **West Merthyr Reclamation Scheme: Environmental Statement, Archaeological Assessment** GGAT (Contracts) Report No 97/014 for Celtic Energy Ltd.



## 2.0 ASSESSMENT METHODOLOGY

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- 2.1 An assessment of the archaeological interests of the proposed development area has been carried out by GGAT (Contracts Division). The work included consultation of the Mid Glamorgan Sites and Monuments Record held by GGAT (Curatorial Division), the RCAHM(W), the Central Registry for Air Photography in Wales, and other published and documentary sources, followed by a walkover survey and appraisal of significance of existing conditions.

### 2.2 Previous studies

A number of recent archaeological studies of the assessment area have been undertaken. The results of the previous assessment covering the south-east portion of the application area (GGAT (Contracts) Report No 97/014) have been incorporated into the present report, and are cross-referenced to the present Project Site numbers in Appendix One. A survey of the Cwm-Glo region on the west side has also been carried out by the RCAHM(W) as part of its Uplands Initiative; the results of the survey have, likewise, been incorporated into the present study, and also cross-referenced to the listed sites in Appendix One. Finally, a comprehensive field survey of the entire area has recently been carried out on behalf of the RCAHM(W) by Cambrian Archaeological Projects.<sup>1</sup> The detailed catalogue of sites for the latter work has not yet been conveyed to the county Sites & Monuments Record, but a copy of the summary and maps has been made available by courtesy of the RCAHM(W). The site numbers of this survey have not been cross-referenced to those in the present report. There have, in addition, been surveys of other areas on the periphery of the study area, including work undertaken by GGAT (Contracts) at Rhyd-y-Car, whose results have been incorporated into the county Sites & Monuments Record.

As there are now several overlapping surveys, and numerous sets of otherwise unrelated site numbers (some of which refer to the same site, others to different *parts* of the same site), it was decided to allocate a new set of Project site numbers (with a suffix MV-) for all sites included in the present study, but to cross-refer these in Appendix One to the earlier sets of site numbers.

### 2.3 Archaeological Areas

For the purposes of the present assessment, the study area as a whole has been subdivided into eight Archaeological Areas. These broadly reflect groupings of similar landscape/site types. The defined Archaeological Areas do not have precise, exclusive boundaries, and individual features (particularly tramroads) tend to cross from one into the next.

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<sup>1</sup> Frost, P & Scott Jones, R 2000 *Uplands Survey Project: West Merthyr Tydfil*. Cambrian Archaeological Projects Report No 119 (Uplands Initiative Field Projects 1999/2000, funded by RCAHM(W)).

## 2.4 Site listings

Within each Archaeological Area, each individual site has been listed according to its site identification number(s); grid reference; site name and a brief description, followed by an assessment of its likely archaeological significance.

Identified sites are listed in Appendix One. The site number prefixes derive from the individual data collections in which the sites have been listed, as follows:

**MVXXX:** Project numbers for all sites in the study area; these are the catalogue numbers used in the location maps.

**PRN XXXX:** Sites referred to by their Primary Record Number (PRN) are incorporated in the county Sites and Monuments Record.

**NPRN XXX:** Sites contained in the National Monuments Record, held by the RCAHM(W) at Aberystwyth.

**WMXX:** Sites previously identified in the 1997 assessment undertaken for Celtic Energy (Report No 97/014).

For the present report, individual sites are referred to by their project number (MVXXX); the catalogue (Appendix One) lists any other identification numbers which have been given to individual sites.

## 2.5 Evaluation

Existing interests have been defined through desk-based searches, these are listed in Appendix One with an assessed category of archaeological value. The sites have been categorised in line with the Department of Transport/Welsh Office/Scottish Office Design Manual for Roads and Bridges Vol 11 Section 3 part 2, according to the following classifications:

- Category A National importance
- Category B Regional importance
- Category C Local importance
- Category D Low importance
- Category E Unknown importance (evaluation required)

These categories have been assigned using the following significance criteria for each site, based on those defined by English Heritage in *Management of Archaeological Projects* (MAP2)<sup>1</sup>.

- Survival/condition
- Period
- Rarity
- Fragility/vulnerability
- Documentation

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<sup>1</sup>English Heritage 1991 Management of Archaeological Projects, 28.



- Group value
- Potential

The HLW (MGL) 2 status is reflected in the categorisation of sites where the group value of individual sites is taken into account in addition to other criteria: these sites include for example the main railways (PRNs 2463m, 2500m, 2858.0m, D38, D39), which form part of the extensive and complex transport system of the area, and the cluster of mining remains at Cwm Pit (2861.0m and 2821m). Note that in many cases, the sites are overgrown or tipped upon, or otherwise not readily accessible. For this reason, the classifications should be regarded as provisional rather than absolute.

- 2.6 The entire area of the development proposals lies within the boundaries of the Merthyr Tydfil designated landscape area of historic interest as outlined in the non-statutory Register of Landscapes, Parks and Gardens of Special Historic Interest in Wales part 2:1 Landscapes of Exceptional and Great Historic Interest produced by CCW, Cadw: Welsh Historic Monuments and ICOMOS UK. The area HLW (MGL) 2 is considered to form part of a "potent example of an internationally renowned industrial landscape of the 18th and 19th centuries and a permanent reminder of man's exploitation of the landscape".
- 2.7 Three sites are currently protected as Scheduled Ancient Monuments in accord with the provisions of the *Ancient Monuments and Archaeological Areas Act 1979*. Two of the Scheduled sites are features of the Cyfarthfa Canal: PRNs 2412 and 2413; both covered by SAM Gm467 (see Table 1 for further information). The third Scheduled monument is a colliery airshaft near Cwm-du Drift Colliery, under SAM Gm460. One Listed structure, the Rhyd-y-Car Skew Bridge (PRN 2425m) lies in the east part of the study area.
- 2.8 The Merthyr Tydfil Borough Local Plan (Deposit 1996) recognises the importance of a number of interests within the assessment area. These are: the Scheduled sections of the Cyfarthfa Canal (Schedule BH 1); all remaining railway lines, the Cwm Colliery Site, the Glamorganshire Canal and the Cwm Woods section of the Cyfarthfa Canal (Schedule BH 4). The local plan also recognises the role of the Sites and Monuments Record.

## **3.0 ARCHAEOLOGICAL BACKGROUND**

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### **3.1 Topographical and geographical setting**

The proposed development is located on the west side of Merthyr Tydfil, lying chiefly to west of the A470 trunk road, and covers an area of about 1.6km<sup>2</sup>, centred on NGR SO 043 050. On its west side, the study area follows a sheltered upland stream valley (the Nant Cwm Glo), lying between 240m and 380m OD. On its east side, the area includes the western slopes of the Taff valley, at between 180m OD and 270m OD. At its southern limit, the area extends to the Nant Canaid gorge. A smaller detached parcel (Archaeological Area 8) lies on the Taff valley floor to east of the A470 at Rhyd-y-Car.

The underlying geology comprises deposits of the Lower Coal Measures, grey silty mudstones with a few sandstones predominate; also contained within the Lower Coal Measures are ironstone deposits comprising bands of ironstone nodules (Blandford 1981, 54). The land is presently used mostly for grazing and includes upland moorland, mature and semi-mature woodland and scrub. There are numerous abandoned coal and ironstone workings in the area and a significant proportion of the landscape is covered with tips.

### **3.2 Prehistoric (up to 1st century AD)**

Although no prehistoric remains have been identified within the area of interest itself, the discovery of a flint scatter south of Abercanaid (PRN 880m) indicates that human activity in the area dates at least to the Mesolithic period. The find of a "thin butted axe-head of siliceous ash" (PRN 478m; GCH 1984, 438) near Twynyrodyn, Merthyr suggests that the area saw continued activity into the Neolithic period.

Cairn-groups ranged along the ridge of Mynydd Aberdare and Mynydd Myrthyr, such as Garn Las (PRN 487m) and Carn Castellymeibion (PRNs 489m - 491m) suggest extensive occupation by the Bronze Age. Settlement of the area continued during the Iron Age; the nearest known site of the period, the Hillfort of Gwersyll (PRNs 483m-485m) lies approximately 1.3km to the east of the reclamation area.

### **3.3 Roman (1st - 4th centuries AD)**

The site of the Roman fort at Penydarren (PRN 823m) lies approximately 1.5km to the northeast of the area of interest. Established in c. 75 AD and abandoned during the 2nd century AD, the fort provided a link between the fort at Cardiff and that at Brecon Gaer. A civilian extramural settlement has been located by excavation and traces of a cemetery have been uncovered extending to the northeast, (GCH 1984, 438; Jarrett 1969, 106; RCAHMW 1976, 84-6).



### 3.4 Early Medieval/Medieval (5th - 16th centuries AD)

Merthyr Tydfil was a centre of some importance during this period as is indicated by the association with Tydfyl, the supposed daughter of Brychan Brycheiniog, a local fifth century king or chieftain (Watkins 1981, 170). More tangible evidence comes in the form of an inscribed stone of sixth century date (PRN 454m) dedicated to Anniccus, son of Tecurus. The stone, now preserved in St. Tydfil's Church, originally stood to the north of the town. A memorial stone bearing the inscription ARTBEU and dated to the eighth or ninth centuries is preserved nearby (PRN 455m; GCH 1984, 438; RCAHMW 1976, 40).

Before the Norman conquest, the Kingdom of Morgannwg was divided into seven *cantrefi*. Merthyr Tydfil was situated within the Commote of Senghenydd Uwch Caiach within the Cantref Breiniol, or the Cantref of Senghenydd. The Welsh chieftain, Ifor ap Meurig, particularly known for holding William Fitzcount, the Earl of Gloucester and his family hostage over a land dispute, held sway over Senghenydd during the mid-twelfth century. The descendants of Ifor ap Meurig retained power in the area until the last quarter of the thirteenth century. Following the Norman conquest the area became part of the demesne lands of Gilbert de Clare, the Earl of Gloucester and Lord of Glamorgan. (Watkins 1981, 172-7)

### 3.5 Post Medieval (16th century to the present day)

Agriculture remained the main industry in the parish of Merthyr Tydfil during the sixteenth century with sheep and cattle farming predominating: over half of the Tithe for the Parish of Merthyr Tydfil in 1535 was derived from sheep and cattle, with corn accounting for less than a quarter (GCH 1974, 3), the presence of a fulling mill, Melin Ganaid, is again indicative of the importance of the woollen industry to the area. The reliance on dairy farming continued into the 17th and 18th centuries: it is known for instance that during the period 1688-1725 farmers in the Merthyr area each on average owned 15 cows (GCH 1974, 323). In 1696 Merthyr is described as 'a village of about 40 houses' and a population of 110 inhabitants has been conjectured; by 1801 the population of Merthyr had increased to 7,700 becoming the largest town in Wales. The effects of rapid industrialisation caused the population to increase fivefold by 1841 (GCH 1974, 372).

The extraction of coal and ironstone during the 18th and 19th centuries is likely to have been far more extensive along the upper reaches of the River Taff, than has previously been thought. It is known that the early iron industry flourished on the west side of the Taff during the 17th century; coal and possibly ironstone extraction in the study area may have originated during this period. The farm name Cwm y Glo (*Coal Valley*) had been established by the early 17th century. A grant of 1619, giving a certain Phillip Williams 'the right to dig coal and quarry stone in all the common lands or forests in Senghenydd Supra...' suggests that small scale workings were common place at the time (Richards 1981, 218-9).

The development of the coal and ironstone workings within the reclamation area is associated with the resurgence in the iron industry seen during the latter half of the 18th century, a direct result of technical innovation, which enabled coke to be used in iron smelting. Dowlais was the first ironworks using the new puddling process to be established in the Merthyr region; the ironworks at Cyfarthfa (PRN 1169m) was established soon after in 1765 by Anthony Bacon. The Cyfarthfa Ironworks drew its raw materials from the coal and ironstone workings on land leased from the Dynevor estate including those workings located in the reclamation area; belonging to this period is the Cyfarthfa Canal (PRNs 2412m/2413m etc), built in the late 1770s to transport coal direct from levels to the ironworks. The Ironworks together with the coal and ironstone workings began a rapid period of expansion under Richard Crawshay, who leased Cyfarthfa in 1786; this early success was in part due to the construction in 1794 of the Glamorganshire Canal (PRN C04m).

While the early extraction industry largely depended on surface workings using a mixture of patching (a process which scours the land of its topsoil), small pits, and where the topography allowed levels driven into the hillsides (Osborne 1976, 41), the real boom years in the industry occurred during the 1820s and 1830s and it was during this period that the deep pits, such as Robbins pit (2826m), Glyndyr's Pit (2816m), Colliers' Row Pit (2811m) and Cwm Colliery (2821m) began to be sunk in the area. The main means of haulage within the shafts was the water balance system, which depended on a plentiful and constant supply of water (Thomas 1981, 306-308); several of the reservoirs in the study area are likely to be relict features associated with this system. The extraction of ironstone and coal in particular continued throughout the nineteenth century, fluctuating with economic demand. By the end of the 1920s the area was largely derelict (Thomas 1981, 330), though there appears to have been a brief period of mining activity during the Second World War.

From about 1840 the development of a complex network of tramroads and railways began, first transporting coal and iron ore to the Cyfarthfa Ironworks and later serving wider markets: the Ynys-fach (D38m), Gethin (2463m), Cwm Pit (2821m) and GWR (Merthyr Branch) (2500m) Railways are the main examples in the study area.

The present appearance of the landscape in the study area is largely due to the rapid growth of the iron and coal industries in the region during the 19th century. Most of the sites are directly associated with coal and ironstone extraction and the transport system which developed alongside it. While there is significant evidence of earlier industrial activity from the surrounding region dating back to the Middle Ages, within the reclamation area such evidence appears to have been removed or obscured by the later intensive industrial land use.



**Table 1: Sites outside the study area**

The following is a list of sites recorded on the Sites and Monuments Record, lying in the vicinity of the study area:

<i>PRN No</i>	<i>Date</i>	<i>Type</i>	<i>Name/description</i>	<i>NGR</i>
CO4m	1794	Canal	Glamorganshire Canal	near SO 0471 0571
454m	Early Medieval	Inscribed stone	The Anniccius Memorial Stone	SO 0496 0582
455m	Early Medieval	Inscribed stone	The Artebeu Memorial Stone	SO 0496 0582
478m	Neolithic	Find	Axe head	SO 0553 0599
483m	Iron Age	Hillfort	Gwersyll Hillfort	SO 0270 0403
487m	Bronze Age	Earthwork	Round barrow	SO 0290 0385
489m	Bronze Age	Earthwork	Round barrow	SO 0345 0310
490m	Bronze Age	Earthwork	Round barrow	SO 0354 0313
491m	Bronze Age	Earthwork	Round barrow	SO 0347 0313
823m	Roman	Fort	Penydarren Roman Fort	SO 0503 0678
880m	Mesolithic	Finds	Flint scatter	SO 050 040
1169m	18th-19th century	Buildings: Ironworks	Cyfarthfa Ironworks	SO 038 069

## 4.0 SOURCES

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### Primary Sources

The records held by Glamorgan Record Office and the National Library were consulted; of particular relevance were the Tithe apportionment for the parish of Merthyr Tydfil, the Land Tax Assessments for the hamlet of Gelli-deg, the papers of the Dynevor, Bute and Gnoll estates and the Dowlais Iron Company papers.

Cartographic Sources consulted comprised:-

Map of the County of Glamorgan, George Yates (1799)  
1: 63,360 Ordnance Surveyors Drawing No. 192 (1814)  
1" to the mile 1st Edition OS map (David & Charles Edition c. 1838)  
8 chains to the mile Tithe map and apportionment for the Parish of Merthyr Tydfil (1850)  
24" to the mile OS Public Health Act Plan of Merthyr Tydfil (1852)  
1:10560 3rd Edition OS map Glamorgan Edition Sheet 12NW (1918)  
1:10560 3rd Edition OS map Glamorgan Edition Sheet 12SW (1922)  
1:2500 1st Edition OS map Glamorgan Edition Sheets XII.5 (1876) and XII.9 (1881)  
1:2500 2nd Edition OS map Glamorgan Edition Sheets XII.5 and XII.9 (1900)  
1:2500 3rd Edition OS map Glamorgan Edition Sheets XII.5 and XII.9 (1919)

### Secondary Sources

Blandford, M	1981	"The Coal Measures" in <i>Merthyr Tydfil: A Valley Community</i> . Merthyr Teachers Centre Group, Cowbridge
Frost, P & Scott Jones, R	2000	<i>Uplands Survey Project: West Merthyr Tydfil</i> . Cambrian Archaeological Projects Report No 119 (Uplands Initiative Field Projects 1999/2000, funded by RCAHM(W)).
GCH	1974	<i>Glamorgan County History. Vol. II Early Glamorgan Prehistory and Early History</i> . Glamorgan County History Trust Ltd., Cardiff
GCH	1980	<i>Glamorgan County History. Vol. IV Early Modern Glamorgan: From the Act of Union to the Industrial Revolution</i> . Glamorgan County History Trust Ltd., Cardiff
GCH	1984	<i>Glamorgan County History. Vol. V Industrial Glamorgan: From 1700 to 1970</i> . Glamorgan County History Trust Ltd., Cardiff
Jarrett, MG	1969	<i>The Roman Frontier in Wales</i> . University of Wales Press, Cardiff
Merthyr National Union of Teachers	1932	<i>The Story of Merthyr Tydfil</i> Merthyr Tydfil
Morris, JH and Williams, LJ	1958	<i>The South Wales Coal Industry: 1841-1875</i> . University of Wales Press, Cardiff
Osborne, B	1976	"Patching, Scouring and Commoners: The Development of an Early Industrial Landscape" in <i>Industrial Archaeology Review</i> Vol. 1, No. 1



Pedlar, FJ	1930	<i>History of the Hamlet of Gellideg.</i> Merthyr Tydfil
Perkins, J,	1986	<i>The historic Taf Valleys, Vol 3: From the Taf confluence at Cefn-Coed-y-Cymmer to Aberfan.</i> Merthyr Tydfil & District Naturalists' Society.
Thomas, C & Evans, J		
RCAHMW	1976	<i>An Inventory of the Ancient Monuments in Glamorgan. Vol. I: Pre-Norman. Part I: The Stone and Bronze Ages.</i> HMSO., Cardiff
RCAHMW	1976	<i>An Inventory of the Ancient Monuments in Glamorgan. Vol. I: Pre-Norman. Part II The Iron Age and Roman Occupation.</i> HMSO., Cardiff
RCAHMW	1976	<i>An Inventory of the Ancient Monuments in Glamorgan. Vol. I: Pre-Norman. Part III: The Early Christian Period.</i> HMSO., Cardiff
Richards, WM	1981	"The Late Fifteenth and Sixteenth Centuries" in <i>Merthyr Tydfil: A Valley Community</i> Merthyr Teachers Centre Group, Cowbridge
Thomas, C	1981	"Industrial Development to 1918" in <i>Merthyr Tydfil: A Valley Community.</i> Merthyr Teachers Centre Group, Cowbridge
Watkins, H	1981	"55BC-1485AD" in <i>Merthyr Tydfil: A Valley Community.</i> Merthyr Teachers Centre Group, Cowbridge
William, E	1987	<i>Rhyd-y-car: A Welsh Mining Community.</i> National Museum of Wales, Cardiff
Wilkins, C	1903	<i>History of Merthyr Tydfil.</i> Cardiff

## Air photographs

The following photographs were examined in the collections of the Central Register of Air Photography for Wales, National Assembly for Wales:

WO Lib No	Sortie No	Source	Date flown	Scale	Type	Prints
4709	CPE UK 1997	RAF	13-4-1947	1:9800	B&W <sub>r</sub>	1359-61; 3330-27
5112	58 RAF 676	RAF	12-5-1951	1:10000	B&W	4407-8; 3406-7
5504	58 RAF 1715	RAF	14-4-1955	1:10000	B&W	181-3
6711	Meridian 71/67	Meridian	23-7-1967	1:5000	B&W	117-20; 125-9
7170	OS 71 208	OS	14-6-1971	1:7200	B&W	61 + 67
7313	Meridian 28/73	Meridian	14-7-1973	1:7500	B&W	167-8
7596	OS 75 313	OS	6-7-1975	1:8200	B&W	88-9
7935	OS 79 059	OS	27-5-1979	1:8200	B&W	001 + 17-18
8501	JA Story 0985	JA Story (NRSC)	28-4-1985	1:20000	Colour	0985: 16-17. 133-4
9035	900525	Carto Services	24-4-1990	Various	B&W	8199-200; 8725-32; 8185-90; 8045-52
9139	H 010	Geonex (NRSC)	1-8-1991	1:10000	Colour	39-40
9626	OS 96 564	OS	8-5-1996	1:9100	B&W	188691: 189-92

## APPENDIX ONE: CATALOGUE OF SITES WITHIN THE STUDY AREA

Sites are listed (a) in order of Archaeological Areas, and (b) in grid reference order, from west to east and from south to north. Within each Archaeological Area, all the point-specific sites (identified by single grid references), appear first, followed by the linear features (identified by terminal references)

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
<p><b>Archaeological Area 1: Upper Cwm Glo</b>            Characterised by scattered coal levels and extensive tipping on the slopes of the Cwm Glo stream valley; the largest feature, the Cwm-du Colliery, remained in production until after the Second World War.</p>								
MV001		85,787		SO 0262 0526	Quarry	Later 19th century, with early 20th century extension	Quarry with extensive waste tips in Upper Cwm Glo. Not on 1850 Tithe map, but shown in 1876 OS 25" 1st ed sheet, possibly already disused by that stage. The quarry was extended on its S side after the 1920s, obscuring most of the S side of the older feature, including the tramroad connecting further waste tips to the quarry.	D
MV002				SO 0263 0517	Coal level	Later 19th century	Coal level on S side of Quarry MV001, immediately outside the boundary of the study area, with associated waste tips at SO 0265 0517 within the study area. Not on 1850 Tithe map, but shown in 1876 1st ed OS 25" sheet, possibly still in use at that stage. Site destroyed by the extension of the quarry after 1921.	D
MV003				SO 0278 0510	Waste tips and tramroad	Later 19th century	Waste tips located 80m to SE of Quarry MV001, and connected to it by a tramroad, extending from SO 0268 0524 to SO 0274 0516. Shown on 1876 1st ed OS 25" sheet, possibly already in disuse. The waste tips may have been an extension of Quarry MV001, or a secondary quarry. Waste tips and tramroad partly obscured by later extension of Quarry MV001.	D
MV004				SO 0273 0549	Cottages site	Later 19th century	Site of pair of cottages with surrounding enclosures on W edge of the study area, near track from Heolgerrig village. Not on 1850 Tithe map, but shown on 1876 1st ed OS 25" sheet. Cottages in ruins by the 1960s.	C
MV005				SO 0273 0560	Cottage site	Post-medieval	Site of cottage with enclosure adjoining Incline MV045. A cottage with enclosure are shown at this position in the 1850 Tithe map, though the building appears to be on a different alignment to that shown on the 1876 1st ed OS 25" sheet. It is possible that an older cottage had been extended or rebuilt after the construction of the incline in the later 19th century. The later building was aligned NW - SE, and approx 14m x 5m in plan, probably including outbuildings.	C



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							The enclosure is 52m x 26m, and is bounded on its N side by the incline.	
MV006				SO 0275 0554	Cottage site	Post-medieval	Site of cottage with enclosure N of Cwm-du Cottage. Shown on 1850 Tithe map, and 1876 1st ed 25" OS. In ruins by 1900 2nd ed OS 25" sheet. Site probably consisted of a small cottage (approx 5m x 7m) at the W (upper) end of the enclosure, with outbuilding(s) extending to E, set in an enclosure, approx 33m x 21m in area.	C
MV007		85,763		SO 0278 0550	Coal shaft	Later 19th century	Coal shaft in Upper Cwm Glo. Not on 1850 Tithe map. Shown disused in 1900 2nd ed OS 6" sheet. Waste tips further downslope to E at SO 0282 0549 appear on the 1st ed OS 6" (surveyed 1875).	D
MV008				SO 0280 0520	Cwm-du Cottage site	Post-medieval	Site of cottage or small farmstead with enclosure, shown on 1850 Tithe map and 1876 1st ed OS 25" sheet. The building was aligned NE - SW, and about 14m x 6m in plan. It probably consisted of a house at the SW (upper) end, with an adjoining barn/byre, set in a farmyard of 50m x 26m, with funnel-shaped access to the unenclosed upland pasture to W. This access was blocked by later 19th century quarrying (MV001). By the 1900 2nd ed OS 25" sheet, the outbuilding was apparently in ruins. The site is now largely obscured by waste tips.	E
MV009		85,783		SO 0282 0543	Coal level	Later 19th century	Coal level, with stone-arched portal. Not on 1876 1st ed OS 25", but shown disused in 1900 OS 25" 2nd ed sheet.	D
MV010		85,782		SO 0282 0546	Coal level	Later 19th century	Coal level, with linear cutting of approx 21m length. Shown disused in 1900 OS 25" 2nd ed sheet. Pit further upslope at SO 0278 0547	D
MV011				SO 0283 0524	Coal level	Later 19th century	Coal level downslope from Cwm-du Cottage site. Shown, apparently disused, in 1876 OS 25" sheet. Site consists of a cutting 20m in length, extending NE, with adjacent shallow waste tips 30m downslope of level mouth.	D
MV012				SO 0283 0552	Waste tips	Later 19th century	Waste tips probably representing shallow coal workings, shown on the 1876 1st ed OS 25" sheet. This shows an area of tips occupying an area of 60m NS x 40m EW, alongside a NS trackway.	D
MV013		85,777		SO 0283 0557	Coal level	Later 19th century	Coal level at Upper Cwm Glo. Not on 1850 Tithe map but is shown disused by 1900 2nd ed OS 25" sheet. The 1st ed OS sheet shows a track leading N from the workings at SO 0282 0549 past the site of the level, which may have been worked by that stage. The 1900 2nd ed 25" OS shows a different track extending NE (probably an abandoned tramroad) connecting the level with the main haulage tramroad to Cyfarthfa, with waste tips on each side. A leat drains the level to E, with a pond at SO 0286 0556.	D
MV014		85,729		SO 0284 0534	Pond	Later 19th	Reservoir in Upper Cwm Glo, approx 60m x 21m in area, with an	C

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
						century	embankment on its E side. Fed by leat supplied by coal levels further upslope. Sluice in embankment on E side, with a leat extending NE. Pond is not shown on the 1850 Tithe map, but appears on the 1876 1st ed OS 25". Its purpose is uncertain, but it may have supplied a balance hoist for the adjacent colliery pit MV018 at SO 0286 0537.	
MV015		85,776		SO 0285 0572	Coal shaft	Later 19th century	Coal shaft in corner of field S of Pen-yr-Heolgerrig, lying just to the N of the development area. Not on 1850 Tithe map; waste tips on 1st ed OS 25" sheet; shown disused on 1900 2nd ed OS 25". Adjacent pits in field to W at SO 0280 0569 are probably related. Waste tips extending N and NE centred at SO 0285 0580. Additional tips lying within the study area centred at SO 0292 0574, on the E side of the tramway. Air shaft MV026 at SO 0297 0572 may also be part of these workings.	D
MV016		85,773		SO 0286 0504	Coal level	Later 19th century	Coal level at Upper Cwm Glo, lying just to the S of the study area. Shown on 1876 1st ed OS 25" sheet, apparently disused. Site consists of cutting 15m in length, with issuing watercourse. Waste tips extend downslope within the study area, centred on SO 0287 0506.	D
MV017				SO 0287 0513	Coal level	Later 19th century	Coal level 100m to SE of Cwm-du Cottage site. Shown, apparently disused, in 1876 1st ed OS 25". 1900 2nd ed 25" sheet shows the level with a short cutting, 14m in length, with an issuing watercourse. The site is now largely obscured by waste tips.	D
MV018		85,730		SO 0287 0538	Colliery pit	Early 19th century	Colliery pit in Upper Cwm Glo. Shown in active use in the 1850 Tithe map, apparently with two shafts. Tramroad or plateway extended N through Heolgerrig village towards Cyfarthfa, with waste tips on E side of tramroad. Colliery disused in 1876 1st ed OS 25" sheet. Described as 'Old Coal Shaft' in the 1900 2nd ed OS 25" sheet.	C
MV019				SO 0289 0527	Possible coal level	Early 19th century	Site of possible coal level cutting extending N downslope towards Tramroad MV048. Lies on the E side of Quarry MV020, where it forms a linear hollow. This feature appears on the 1876 1st ed OS 25" sheet, apparently pre-dating the quarry. Near the lower end of the hollow, a series of small lobed tips extends from the E (opposite) side of the tramroad at SO 0292 0533; also shown on the 1st ed OS 25" sheet. The linear hollow may have been an early coal level cutting (of which there are several examples in the vicinity), but could, alternatively, have been the first stage of the quarry cutting.	D
MV020		85,785		SO 0289 0529	Quarry	Late 19th century	Quarry in Upper Cwm Glo. Not on 1876 1st ed OS 25", but shown in active use in 1900 2nd ed OS 25". By that stage the quarry had a worked face of about 55m. A short quarry tramroad connected the quarry floor with the existing colliery tramroad MV048, leading N	D



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							through Heolgerrig village towards Cyfarthfa. It is possible that the quarry had originated as a cutting for a level (on the S side of the quarry), which is shown on the 1st ed OS 25".	
MV021		85,784		SO 0289 0543	Ventilation shaft	Later 19th century	Air shaft at Upper Cwm Glo, to E of tramroad. Not on 1876 1st ed OS 25", but shown in 1900 2nd ed OS 25" sheet. Probably part of the Cwm-du Drift colliery workings.	D
MV022		85,788		SO 0290 0522	Level	Later 19th century	Level in Upper Cwm Glo. Shown disused in 2nd ed OS 25" sheet (though not marked as a level). Consists of a shallow pit on the W side of Tramroad MV048 with waste tips extending to N and E.	D
MV023				SO 0292 0540	Waste tips	Early 19th century	Waste tips extending over area of 120m NS by 100m EW, on E side of plateway. Derived from coal shaft MV018, which appears on the 1850 Tithe map.	D
MV024		85,764		SO 0292 0548	Ventilation shaft	Later 19th century	Air shaft in Upper Cwm Glo, to E of tramroad. Not on 1876 1st ed OS 25", but shown in 1900 2nd ed OS 25" sheet. Probably part of the Cwm-du Drift colliery workings.	D
MV025		85,789		SO 0293 0516	Level	19th century	Level in Upper Cwm Glo. Not on 1876 1st ed OS 25", but shown in disused state in 1900 2nd ed OS 25". Site consisted of a short, narrow cutting, 14m in length, with small, shallow waste tips downslope. A pit, probably the result of collapse, lay slightly upslope of the level mouth. Given the limited scale of the workings, this may have been a trial level.	D
MV026		85,723		SO 0297 0572	Ventilation shaft	Later 19th century	Air shaft shown on 1st (1876) and 2nd ed (1900) OS 25" sheets. Disused by 1914. Probably part of the workings from Shaft MV015 at SO 0285 0572.	D
MV027				SO 0298 0539	Cwm-du Drift portal	Late 19th century	Cwm-du Drift portal, located in a cleft between the older waste tips from the early 19th century shaft mine to W, with which the drift is probably linked. Not shown on the 1876 1st ed OS 25". The 1900 OS 25" sheet shows the colliery in use. A main haulage tramroad extends E from the drift mouth, with waste tips extending 140m to NE of the mouth, and several buildings adjacent to the tramroad. The mine was abandoned in about the 1940s, and subsequently converted into a stable yard. (See separate descriptions of individual features).	A
MV028				SO 0299 0539	Cwm-du Drift engine house	Early 20th century	Small colliery engine house located on or adjoining the drift portal. Stone built, with brick-lined arched windows. Not on 1900 2nd ed OS 25", but shown on 1919 3rd ed OS 25". Has been re-roofed and now serves as part of a stable yard.	A
MV029				SO 0299 0540	Cwm-du Drift blacksmiths' shop and stable	Late 19th century	Narrow, single-storeyed building, approx 12m x 5m, to NE of the engine house at Cwm-du Drift. Has several window and door openings; its purpose is uncertain, though it may have been a	A

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							blacksmiths' shop and stable. A pond, approx 18m length, lay to N. This is one of two colliery buildings shown on the 1900 2nd ed OS 25" sheet. Now serves as part of a stable yard.	
MV030		85,781		SO 0299 0560	Coal level	Later 19th century	Level at Upper Cwm Glo. Not on 1876 1st ed OS 25" sheet but shown recently disused in 1900 2nd ed OS 25". Site consisted of a level mouth (probably stone-arched), with a cutting to E, 20m in length. A small shed lay on the N side of the cutting. Waste tips extended to E, to a distance of 112m from the level mouth.	D
MV031		85,790		SO 0300 0529	Coal level	Later 19th century	Coal level at Upper Cwm Glo. Not on 1876 1st ed OS 25", but shown disused in 1900 2nd ed OS 25". Site consisted of a cutting 30m in length, with shallow waste tips extending to E to a distance of 65m from the level mouth. A linear pit 50m to W of the level mouth probably represents collapse of the roof. Partly encroached upon by waste tips from Cwm-du Drift.	D
MV032				SO 0304 0540	Cwm-du Drift colliery building	Late 19th century	Building located 40m to E of the engine house, on N side of main haulage tramroad. Now a group of brick sheds of different periods, forming part of the stable yard. Shown on 1900 2nd ed OS 25" sheet as a square building, 10m x 10m, with an adjoining branch tramroad on its N side.	A
MV033		85,724		SO 0305 0498	Coal level	Later 19th century	Coal level at Upper Cwm Glo, lying just to the S of the study area. Shown in use in 1876 1st ed OS 25" sheet. Disused by 1900 2nd ed OS 25" sheet. Site consisted of a cutting 19m in length, with a watercourse issuing and draining to one of two ponds 90m to NNE at SO 0309 0507. The 1st ed OS 25" shows a tramroad extending from the level mouth to NW; the trackbed is still extant, and forms the S end of Tramroad MV048. It seems likely that the waste was tipped on the E side of this tramroad at SO 0300 0514.	D
MV034		85,805		SO 0306 0506	Level	Later 19th century	Level at Upper Cwm Glo. Shown disused in 1900 2nd ed OS 25". Site consisted of a cutting of approx 27m length, with an issuing watercourse. Waste tips extend to the NW, with a trackway through the tips probably the bed of a tramroad connecting the level with the haulage tramroad 85,673.	D
MV035	2421m	85,728		SO 0315 0519	Ventilation shaft	Later 19th century	Fan and air shaft shown in 1876 1st ed OS 25". Rare survival of small colliery ventilation shaft and fan. Scheduled Monument SAM Gm460	A
MV036				SO 0315 0545	Cwm-du Drift waste tips	Late 19th - mid 20th century	Extensive lobed colliery waste tips extending to E of Cwm-du Drift, covering an area of 350m EW x 270m NS, on both sides of the main haulage tramroad.	B
MV037		85,794		SO 0318 0500	Shaft	Later 19th century	Shaft at Upper Cwm Glo, slightly upslope of coal level MV038. Shown disused in 1900 2nd ed OS 25". This feature is presumably	D



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							associated with the adjacent level mouth.	
MV038		85,793		SO 0319 0501	Level	Later 19th century	Level at Upper Cwm Glo. Shown in use in 1876 1st ed OS 25" sheet, but disused in 1900 2nd ed OS 25". Site consists of a cutting 42m in length, extending N to the tramroad MV049. The 1st ed OS sheet shows a short tramroad extending from the level mouth to the haulage tramroad. Waste tips extend downslope to N of the haulage tramroad, centered on SO 0320 0509.	D
MV039				SO 0334 0524	Possible shaft	19th century	Shallow, oval pit, approx 8m across, located to SW of the Cwm-du Drift main waste tip. Possibly a trial pit. Shown on 1900 2nd ed OS 25" sheet.	D
MV040		85,800		SO 0338 0508	Coal level	Later 19th century	Coal level at Upper Cwm Glo. Shown disused in 1876 1st ed OS 25" sheet. Site consists of cutting, 45m in length, occupied by stream course, with waste tips extending to 67m from level mouth.	D
MV041		85,801		SO 0342 0535	Ventilation shaft	Early 20th century	Air shaft at Cwm Glo. Not on 1900 2nd ed OS 25", but shown on 1914 OS 6". Located close to Cwm-du Drift haulage tramroad, 70m NW of air shaft 85,779.	D
MV042		85,799		SO 0346 0529	Ventilation shaft	Early 20th century	Air shaft at Cwm Glo. Not on 1900 2nd ed OS 25", but shown on 1914 OS 6", 70m SE of air shaft MV041.	D
MV043		85,802		SO 0347 0539	Coal level	Early 20th century	Coal level at Cwm Glo. Not on 1900 2nd ed OS 25" sheet, but shown in 1914 OS 6" revision and 1919 3rd ed OS 25" sheet.	D
MV044		85,798		SO 0352 0527	Coal level	Later 19th century	Coal level at Cwm Glo. Not on 1876 1st ed OS 25" but shown disused in 1900 2nd ed OS 25". Site consisted on a straight cutting, 47m in length, extending NE from the level mouth, with waste tips extending to 90m from level mouth.	D
MV045		85,672		SO 0257 0555 - SO 0289 0569	Incline	Later 19th cent	Incline connecting the extensive waste tips to W of the study area with the older main Cyfarthfa plateway. Not on the 1850 Tithe map but shown active on the 1876 1st ed OS 25" sheet. Disused by 1900 2nd ed OS 25" sheet.	C
MV046				SO 0286 0537 - SO 0342 0657	Plateway/ tramroad	Early 19th century	Plateway or tramroad originally connecting the early 19th century colliery pit MV018 at SO 0286 0537 with the Cyfarthfa coke yards. It followed a route N through Heolgerrig village, over a distance of 1.38km. The plateway or tramroad is shown on the 1850 Tithe map. By the 1876 1st ed OS 25" sheet, the early coal pit MV018 had been abandoned and the tramroad had been extended S (MV048 and MV049) to a series of coal levels.	B
MV047				SO 0298 0539 - SO 0382 0535	Cwm-du Drift main haulage tramroad	Later 19th century	Tramroad connecting Cwm-du Drift with the incline head. Extends W for approx 820m, to the S of Pen-y-Coedcae farm. Not on the 1850 Tithe map, but shown in use in the 1876 1st ed OS 25" sheet, and still in use in the 1919 3rd ed OS 25".	B

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
MV048		85,674 and 85,688		SO 0305 0502 - SO 0295 0518	Coal level tramroad	Later 19th century	Tramroad connecting later 19th century coal levels with older plateway across Cwm Glo towards Cyfarthfa. Follows a fairly straight course SSE - NNW (with a bend at its S end) downslope over a distance of 490m. Is joined by the secondary tramroad 85,673 and by the quarry tramroad []. Tramroad shown in use in the 1876 1st ed OS 25" sheet, but disused by the 1900 2nd ed OS 25" sheet.	C
MV049		85,673		SO 0340 0494 - SO 0294 0524	Coal level tramroad	Later 19th century	Tramroad connecting several later 19th century coal levels with the main tramroad 85,674 across Cwm Glo towards Cyfarthfa. Follows a fairly straight course SE - NW along the contour of the hillslope for some 550m. Shown on the 1876 1st ed OS 25" sheet, apparently still intact as far as SO 0317 0507. Entire length abandoned by the 1900 2nd ed OS 25" sheet.	C
<p><b>Archaeological Area 2:</b> South Heolgerrig/ Nant Llwyn-yr-Eos stream valley  The area on the south side of Heolgerrig Village, with part of the upper Nant Llwyn-yr-Eos stream valley. This area has been relatively less affected by industrialisation and reflects more of the earlier landscape; it includes a cluster of farmsteads or cottages and the site of the late 17th cent Cwm-y-Glo chapel</p>								
MV050				SO 0329 0540	Cwm Glo Uchaf cottage site	Post-medieval	Remains of building shown on 1850 Tithe map, with surrounding enclosures, along the Nant Llwyn-yr-Eos stream. Known as Llwyn-yr-Eos in the 1900 2nd ed 25" sheet. Site consisted of a small building, 9m x 5m, with a few surrounding structures, including a bridge over the stream.	
MV051				SO 0330 0585	Pond or possible patch working	Probably 19th century	Large open-ended rectangular depression located to S of Pen-yr-Heolgerrig. This feature is aligned WSW-ENE, approx 200m x 55m. Its open E end grades into the hillslope, but is partly blocked by a waste tip. Not on the 1850 Tithe map, or the 1876 1st ed OS 25" sheet, but shown on the 1900 2nd ed OS 25" sheet, apparently in disuse. This may be one of several large early 19th century ponds in the vicinity of Heolgerrig, or an early patch working. Note that the E end of the feature lies close to the outcropping coal seam. A similar feature (MV056) lies on the Nant Llwyn-yr-Eos stream, 340m to the SW.	E
MV052	450m			SO 0348 0545	Chapel site	c 1689/90	Foundations of an early Dissenter chapel, thought to have been built in 1689 and in use as a chapel until 1749. Subsequently used as a barn. Not shown on the 1850 Tithe map, and presumably in ruins by that date. Site of building (36 foot x 18 foot) located on the N side of the Nant Llwyn-yr-Eos, 200m downstream of the Cwm Glo Uchaf cottage	B



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							site.	
MV053		85,800		SO 0355 0585	Reservoir	Early 19th century	Large reservoir (170m x 60m; 0.89ha). One of five ponds in the vicinity shown on the 1850 Tithe map. The reservoir is an irregular rectangle, adapted to the existing field layout on its NW corner. It has a stone-revetted embankment on its NE and E sides and a sluice at its NE corner. Leats from this sluice fed smaller reservoirs to NE and SE. The reservoir's primary function is uncertain, but it may have been a feeder for the Cyfarthfa Canal. It lies immediately outside the study area.	B
MV054				SO 0356 0568	Cwm-y-Glo farmstead site	Early post-medieval	Site of small farmstead and surrounding enclosures adjacent to Nant Cwm Glo, reached by access road from Heolgerrig village. The farm was documented in the early 17th century, and is probably late medieval in origin (Thomas, 1981). Cwm-y-Glo farmstead is considered to have been the centre of Dissenter worship in North Glamorgan from 1669 until the erection of the nearby chapel in 1689. As shown on the 1850 tithe map, the farmstead consisted of three main blocks, of which only the W and E blocks were still occupied by the 1876 1st ed OS 25". These in turn fell into ruin after 1919.	B
MV055	2825m		WM29	SO 0365 0559	Gelli-Ddu	Post-medieval	Remains of cottage and outbuildings arranged around central well or pit, at the junction of the Nant Lwyn-yr-Eos and Nant Cwm Glo streams. Shown on the 1850 Tithe map. Ruinous by 1973.	C

**Archaeological Area 3:** Tramroad/plateway and incline corridor, to S of Heolgerrig village

Defined by the tramroad/ plateway as far as the incline, with tips and workings on each side, the Nant-yr-Eos stream gorge and the incline corridor. Four sub-areas: (A) tips at the N end; (B) the Coedcae Pit site and waste tips; (C) the extensive tips along the Nant Llwyn-yr-Eos, derived from Cwm Glo Pit on the N side of the stream and various levels on the hillslope on the S side of the stream; (D) the incline corridor.

MV056				SO 0355 0550	Waste tips	Probably 19th century	Extensive area of waste tips and hollows on the W side of the Nant Llwyn-yr-Eos, covering an area of 130m NZ x 80m EW. The location on the stream valley, close to the outcropping seams, suggests early workings, similar in arrangement to the larger feature MV051, 340m to N.	E
MV057				SO 0366 0550	Ventilation shaft	Later 19th century	Air shaft located in area of workings, 27m to SW of coal level mouth, and possibly associated with it. Shown in 1876 1st ed and 1900 2nd ed OS 25" sheets, but not on 1919 3rd ed 25" OS sheet.	D
MV058	2832m		WM45	SO 0367 0549	Level and Tramway	Later 19th century	Coal level located to SE of Gelli-Du Cottage. Shown on 1876 1st Edition OS map, though not marked as a level. Tramroad extended NW, crossing the Nant Cwm Glo via an embankment (now removed)	D

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							to join the complex of tramroads around Cwm Glo Pit. Apparently disused by 1900	
MV059	2824m		WM28	SO 0370 0538	Pen-y-Coedcae	Early 19th century	Remains of a row of four early 19th century workers' cottages, with associated enclosures. Possibly on the site of (or adapting) an earlier cottage or farmstead, similar to Llwyn-yr-Eos or Gelli Du. The row is 18m x 8m, facing NNE, with gardens to front and rear. It is shown on the 1850 Tithe map. Demolished in about 1970.	C
MV060				SO 0372 0526	Coal level	Mid 19th century	Probable position of a coal level on hillslope to SE of Pen-y-Coedcae cottages, about 20m to NNW of later level mouth MV061 (2836m). The 1876 1st ed OS 25" shows a cutting, 30m in length, with a shallow linear tip extending downslope beyond this to a distance of 62m from the top of the cutting. Although this feature is not labelled as a level, it bears all the characteristics of an early coal level. It remained unchanged in the 1900 2nd ed OS 25" sheet. By the 1919 3rd ed OS 25" sheet, a second level had been opened up immediately to the E of the old one; new waste tips extended to the sides of the old tip and the top of the old cutting had been cut back by some 20m. It seems likely that this was a trial re-opening of the older level; neither level was in use by 1919.	D
MV061	2836m		WM49	SO 0374 0524	Coal level	Early 20th century	Coal level on hillslope to SE of Pen-y-Coedcae cottages, adjacent to earlier level. Waste tips extending downslope to NW to a distance of 63m from level mouth. Not shown on 1876 1st ed or 1900 2nd ed OS 25" sheets. Shown disused in 1919 3rd ed OS 25" sheet. Field visit showed number of associated pits to NW and SE. Possibly a trial re-opening of the mid 19th century level adjacent to it.	D
MV062				SO 0377 0567	Cwm Glo Pit Reservoir	Early 19th century	Rectangular embanked pond on hillslope above Cwm Glo Pit. Irregular rectangle in plan, 50m x 40m (0.11ha), with earthen embankment on its E, S and W sides. Fed by leats from the NW and N (the latter supplied by a larger reservoir), with a sluice at its E corner. One of five ponds in the vicinity shown on the 1850 Tithe map. Probably provided to supply a balance hoist for Cwm Glo Pit.	B
MV063				SO 0377 0592	Reservoir near Coedcae Pit	Early 19th century	Rectangular embanked pond located to W of the main haulage tramroad. An irregular rectangle, 50m x 40m (0.16ha), with embanked NW and NE sides, and a sluice at its E corner. A secondary outfall leat extends NW from its N corner to the Nant Cwm-pant-bach, 100m to N. A substantial channel, perhaps a leat or trackway, is shown on 1st ed OS 25" map, extending from the pond towards the Coedcae Pit, located 80m to the NE. The pond was supplied by leats from other reservoirs to the S and SW. One of five ponds in the	B



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							vicinity shown on the 1850 Tithe map, and very similar in scale and design to the Cwm Glo Reservoir to the S, which supplies it by leat. It may have been provided for the future Coedcae Pit, although it is doubtful whether that pit (sunk after 1875) ever relied on a water-balance hoist.	
MV064				SO 0378 0563	Cwm Glo pond	Early 19th century	Small rectangular pond to south of reservoir. The pond was 25m x 8m, fed by a leat from the W, with an outfall on its E side. The plan suggests that it was stone or brick-lined. The pond was probably provided for Cwm Glo Pit, located 20m to SE. Shown on the 1850 Tithe map and the 1884 1st ed OS 25" sheet. By the 1905 2nd ed 25" sheet, the pond had apparently been backfilled and overlain by a short-lived tramroad. No visible remains of the pond, but the feeder leat can be traced.	D
MV065				SO 0379 0600	Coedcae Pit waste tip	Mid 19th century	Waste tip (150m x 75m) on W side of plateway/tramroad opposite Coedcae Pit, extending S to Coedcae Pit Reservoir. Established after the sinking of Coedcae Pit in the late 1850s. Shown in its present form in the 1876 1st ed OS 25" sheet, and probably already disused by that stage.	D
MV066	2826m		WM30	SO 0382 0561	Cwm Glo Pit (Robbins Pit)	Early 19th century	Site of early 19th century ironstone and coal pit, now a capped shaft with adjacent building platform. Probably established in the 1830s or 40s, and shown in use on the 1850 Tithe map and 1876 1st Edition OS 25" sheet. Disused by 1905 2nd ed OS 25" sheet. The Tithe map shows two buildings: the pit head itself and an adjacent structure on the N side. It is likely to have been powered by a water-balance hoist; two ponds lay to the NW. Haulage from the pit was provided by a plateway or tramroad located immediately to the W (MV098), which connected the pit with the main Cyfarthfa haulage tramroad (MV101). The extensive waste tips to the W and NW of the pit were already well-developed by 1850, as the pronounced eastward detour of the main E haulage tramroad suggests. By 1876, the two older plateways or tramroads had been replaced by a newly aligned NS tramroad to the Cyfarthfa cokeyards (MV099) and by an incline E to the Cwm Pit Railway (MV146).	C
MV067				SO 0383 0600	Coedcae Pit	Later 19th century	Site of later 19th century ironstone pit, located on the E side of the re-aligned haulage tramroad MV099. Not shown on the 1850 Tithe map but working by 1860. The 1876 1st ed OS 25" sheet shows the pit head with a separate building (probably including an engine house) to the W. Coedcae was relatively shallow - 100 yards in depth. Waste tips extended to E and SE. Disused by 1900 2nd ed 25" OS sheet.	C



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							Roofless colliery buildings still substantially intact in the late 1960s, but since demolished. Rubble heaps now overgrown.	
MV068				SO 0384 0605	Coal/clay level	Mid 19th century	Stone-lined level mouth to S of Nant Cwm-pant-bach. Not on the 1850 Tithe map, but shown as a disused coal level on 1876 1st ed OS 25", with tramroad extending along stream valley E to Cwm Pit Railway, and sidings with other structures adjacent to Upper Colliers Row at SO 0398 0610. Described as an old clay level in 1900 2nd ed OS 25" sheet.	D
MV069	2846m		WM59	SO 0389 0540	Level	Mid to later 19th century	Probable level, shown as an apparent disused cutting in 1876 1st ed OS 25" sheet. The cutting is approx 20m in length, with the line of a possible tramroad embankment to NW, leading to the former main haulage tramroad MV101). Fan-shaped waste tips extend to NW, to a distance of 170m from the level mouth, shared with the adjacent level 2833m to SW. (Note: This level was previously described as two linear features, centred at SO 0307 0545). Now flooded.	D
MV070				SO 0390 0590	Coedcae Pit main waste tip	Later 19th century	Extensive waste tip (220m x 190m) lying between the haulage tramroad MV099 to W and the Upper Colliers' Row allotments to E; bounded to N by the Nant Cwm-pant-bach stream valley and to S by the Cwm Glo pit main waste tip MV077. Good example of a level surface tip at Merthyr, produced by tipping in stages along a series of parallel tram lines (shown in use in the 1876 1st ed OS 25" sheet), in contrast to the earlier and less space-efficient 'dendritic' lobed tips. Probably started soon after the sinking of Coedcae Pit in the 1850s or early 1860s. The pit had been abandoned by the 1900 2nd ed OS 25" sheet. Surface of the tip now enclosed as a paddock.	C
MV071				SO 0390 0609	Possible stables and stores for level	Mid 19th century	'L' shaped building located on the line of the former main haulage plateway or tramroad MV 101, where it crossed the Nant Cwm-pant-bach, 50m W of the level mouth. The building was probably constructed in the late 1850s or early 1860s, after the removal of the tramroad. It is shown, apparently in ruins, on the 1876 1st ed OS 25", where it measured 27m x 5m, and was divided into several rooms. The building had disappeared by the 1900 2nd ed OS 25" sheet. It seems likely to have been a stable and stores for the adjacent level, possibly including domestic accommodation; its construction on the line of the former tramroad crossing is of interest.	C
MV072				SO 0390 0620	Coedcae Pit north waste tip	Later 19th century	Extensive waste tip lying between the Nant Cwm-pant-bach and Heolgerrig main street, occupying an area of 220m x 160m. Extended NE from Coedcae Pit and level(s) along the stream. Unlike the Coedcae main tip (MV070), on the S side of the Cwm-pant-bach, this	D

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							tip was characterised by tiers of irregular lobed tip lines. Shown on the 1876 1st ed OS 25" sheet, probably already disused. Has been partly reclaimed.	
MV073	2835m		WM48	SO 0394 0526	Coal level	Early 20th century	Flooded coal level on S side of Incline MV145. Not on 1st or 2nd ed OS 25" sheet, but shown (probably in disuse), on 1919 3rd ed OS 25" sheet.	D
MV074	2833m		WM46	SO 0392 0535	Coal level	Mid to later 19th century	Coal level shown as a disused cutting on 1876 1st ed OS 25" sheet, though not described as a level. The cutting is 60m in length, extending NW to the former main haulage tramroad MV101, with waste tips extending NW of the tramroad, to a distance of 190m from the level mouth (shared with the adjacent level MV069 to the NW). The 1919 3rd ed OS map shows a new level mouth to the side of the older cutting, with an additional small waste tip overlying the earlier tips immediately to NW of the former haulage tramroad. The feature, clearly disused, is labelled as 'Old Coal Level' in the latter map. It seems likely that the old level had been re-opened for a trial period in the early 20th century.	D
MV075				SO 0396 0547	Coal level	Later 19th century	Coal level located 40m to S of the Nant Cwm Glo, immediately to E of the main haulage tramroad. Shown in use on the 1st ed OS 25" sheet, with a tramroad extending NNW, to join the line of the truncated main haulage tramroad MV101 across the Nant Cwm Glo and shorter tramroads to the waste tips. The tips are located along both sides of the stream; on the N side centred at SO 0407 0557, covering an area of 190m x 100m, to a distance of 240m from the level mouth; on the S side centred at SO 0410 0548, to a distance of 180m from the mouth. The level is shown disused in the 1900 2nd ed OS 25" sheet.	C/D
MV076				SO 0395 0551	Culvert	Early 19th century	Position of culvert which carried the early 19th century main haulage tramroad/plateway MV101 over the Nant Lwyn-yr-Eos stream gorge, some 50m downstream of a similar feature. The overlying embankment has been washed away in recent years	D
MV077				SO 0395 0575	Cwm Glo Pit main waste tip	Mid to late 19th century	Extensive colliery waste tip with flattened profile (c 200m x 200m) on E side of main haulage tramroad MV099, extending N to the adjacent Coedcae Pit main tip MV070 and S to the Cwm Glo Pit Incline (MV144). The tip was probably initiated soon after the sinking of Cwm Glo in the 1803s or 40s; its site is described as 'rubbish tips' in the 1850 Tithes apportionment. In its present form, however, the tip is largely a product of the mid to late 19th century; like the Coedcae Pit main waste tip MV070 (which adjoins it to the N), this is a good	C



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							example of a level surface tip, produced by tipping along parallel tramlines. In its initial stages, the tip was probably constrained by the main haulage tramroad MV101 on its E side. By the 1876 1st ed OS 25" sheet, however, the tip had been allowed to encroach on the line of the abandoned tramroad, completely covering it at its SE corner. The tip was disused by the 1900 2nd ed OS 25" sheet.	
MV078	2834m		WM47	SO 0400 0528	Ironstone level	Mid 19th century	Ironstone level located at the S end of main haulage tramroad MV101. Cutting to NW, 15m in length, to tramroad junction. Waste tips further to NW, covering an area of 100m x 100m, to a distance of 145m from the level mouth. Levels are not recorded on the 1850 Merthyr Tithe map, but the extension of the main haulage tramroad to this point shown on the map suggests that the level was in use by that stage. The level is shown disused on 1876 1st Edition OS map. One of a number of workings in the area.	D
MV079				SO 0402 0582	Coal level	Early 20th century	Coal level lying to E of MV101 tramroad embankment. Does not appear on 1900 2nd ed OS 25" sheet; shown in disuse on 1919 3rd ed OS 25" sheet.	D
MV080				SO 0405 0576	Coal level	Early 20th century	Coal level mouth lying to E of MV101 tramroad embankment. Does not appear on 1900 2nd ed OS 25" sheet; shown in disuse on 1919 3rd ed OS 25" sheet.	D
MV081	2849m		WM62	SO 0410 0505 (approx)	Collapsed coal workings	Probably 19th century	Cluster of flooded pits adjacent to footpath MV107. Not shown on 25" OS maps - possibly represents collapsed shallow underground workings?	D
MV082	2837m		WM50	SO 0403 0522	Coal level	Early 20th century	Coal level to the S of Incline MV145. Remains of stone revetment. Shallow waste tips extending E, to a distance of 45m from level mouth. Not on 1st or 2nd OS 25" sheets, but shown in disused state in 1919 3rd ed OS 25" sheet. Waste tips to NW suggest that a second level may have been located in the vicinity.	D
MV083			WM61	SO 0410 0520	Levels	Probably early 19th century	Sites of two adjacent levels adjoining trackway or former tramroad MV106. One level is partly covered by tipping. Surviving stone revetment. Not shown on the OS 25" maps	D
MV084				SO 0412 0575	Coal level	Early 20th century	Coal level adjacent to Cyfarthfa Canal. Not on 1900 2nd ed OS 25" sheet. Shown on 1919 3rd ed OS 25", apparently still in use, with cutting of 10m length, and tramroad extending SE along the length of the former canal towards the Cwm Pit Railway.	D
MV085	2823m		WM27	SO 0413 0525	Pen-cae Cottage	Post-medieval	Remains of stone/brick-built cottage within enclosure. Cottage is approx 7m x 7m, with an outbuilding to SE. Gardens to front and rear. Probably 17th or 18th century in date. Shown on 1st Edition OS 1" (c 1838) and 1876 OS 1st ed 25" sheet. Demolished in about 1970.	C



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
MV086	2829m/ 2838m		WM42/ WM51	SO 0415 0510	Level	Early to mid 19th century	Level, consisting of cutting, 28m in length, extending to trackway or former tramroad MV106, with waste tips extending downslope beyond the trackway to a distance of 70m from the level mouth. Other shallow pits in the vicinity of the level mouth may be trial levels or earlier workings. This is one of several levels adjoining the trackway. Shown disused on 1876 1st ed OS 25" sheet.	D
MV087	2847m		WM60	SO 0416 0531	Ventilation shaft	Early 20th century	Airshaft located to N of the Pen-cae Incline (MV146). Not on 1900 2nd ed OS 25" sheet, but shown on 1919 3rd ed OS 25" sheet.	D
MV088	2799m		WM2	SO 0416 0569	Upper Wern farmstead	Probably post-medieval	Remains of cottage or farmstead located close to Cwm Glo Pit Incline (MV144). Main block is 22m length, probably including a house to the N and adjoining barn or outbuilding to S, with narrow surrounding enclosures. Shown on 1814 OS surveyors' drawing and appears on the 1850 Tithe map as Wern Fach. Additional detached outbuildings in 1876 1st ed OS 25" sheet. Ruined by 1967.	C
MV089				SO 0418 0550	Probable keeper's cottage, Cyfarthfa Canal	1770s	Site of building adjacent to the W side of the aqueduct MV090 carrying the Cyfarthfa Canal across the Nant Cwm Glo stream valley. The building is shown on the 1850 Tithe map as an 'L'-shaped structure, approximately 12m in length and 4m in width, with a small enclosure. It had been largely demolished, apart from its S end, by the 1876 1st ed OS 25" sheet. No traces are shown in the 2nd or 3rd ed OS 25" sheets. This seems likely to have been a canal keeper's cottage associated with the lock on the N side of the aqueduct. The feature is part of the Upper Wern aqueduct complex. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	A
MV090		91,551		SO 0419 0552	Canal aqueduct, Cyfarthfa Canal	1770s	Aqueduct formed by an embankment over a culvert, carrying the Cyfarthfa Canal across the Nant Cwm Glo stream valley. Forms a complex structure integrated with the west gate and embankment of the adjoining canal lock (MV091). A building thought to be the canal-keeper's cottage lay on the W side of the aqueduct, to S of the brook. Shown in outline on the 1850 Tithe map. The 1876 1st ed OS 25" sheet shows most of the arrangement of the complex. The aqueduct has been largely demolished in recent years, though the N end with its junction with the canal lock remains intact, along with part of the S embankment.	A
MV091	2798m		WM1a	SO 0420 0554	Upper Wern Canal Lock, Cyfarthfa Canal	1770s	Remains of early lock on the Cyfarthfa Canal, integrated with the aqueduct over the Nant Cwm Glo brook, and probably constructed to raise the lower water level of the canal on the N side of the stream. The lock was aligned EW, approximately 70m in length and 5m in	A

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							width, and terraced into the slope of the stream valley on its N side, with an embankment on its S side. Its E end is partly buried under the Cwm Pit Railway embankment. The W end of the lock has curving masonry abutments which form the most prominent element today. From this point, the canal was turned through more than 90 degrees and carried at the higher level across the aqueduct over the Nant Cwm Glo. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	
MV092				SO 0424 0509	Possible level	Mid 19th century	Possible level located close to trackway or former tramroad MV106, near Incline MV145. The 1876 1st ed OS 25" sheet shows a watercourse and trackway extending E for 43m, joining a trackway which	D
MV093	2828m		WM36	SO 0425 0517	Airshaft	Later 19th century	Stone-lined airshaft located on N side of Cwm-du Pit Incline (MV145), 57m to W of reservoir MV114. Shown on 1876 1st ed 25" sheet, but not on 1900 or 1919 editions. Feature still visible, retaining part of lining	C
MV094	2844m		WM57	SO 0426 0438	Ventilation shaft	Early 20th century	Airshaft located 40m to SW of Cwmfelin Drift main portal. Not on 1881 1st ed OS 25" sheet, but shown on 1919 3rd ed OS 25", probably in use. Shaft is now about 3m deep, with brick lining and timber beams visible.	C/D
MV095	2839m		WM52	SO 0428 0513	Coal level	Early 20th century	Coal level located to S of the Cwm-du Pit Incline, 30m to SW of reservoir MV114. Tramroad extended NW from level mouth to join the incline tracks as a parallel line to the Cwm Pit Railway coal loader MV126. Branch tramroad crossed the incline and extended N to waste tips on the E side of the reservoir, to a distance of 190m from the level mouth. Not on 1900 2nd ed OS 25" sheet, but shown in active use on 1919 3rd ed OS 25". Field visit identified at least three levels in the vicinity with surviving tramroad bed of tramroad and associated linear tip remains, overlying the older incline MV146.	D
MV096				SO 0435 0504	Ironstone level	Later 19th century	Level located adjoining the N side of the cottage enclosure MV117 (2810m). Short cutting of 10m leading directly to branch haulage tramroad MV102. Shown disused in 1876 1st ed OS 25" sheet. By 1900, a new coal level, MV115 (2840m), had been driven in to the hillslope some 22m to the W.	D
MV097				SO 0365 0545 - SO 0361 0553	Possible tramroad	19th century	Straight, linear earthwork extending NNW from workings near Pen-y-Coedcae to Nant Lwyn-yr-Eos stream. Possibly a section of tramroad. Not on 1850 Tithe map, but shown in disuse in 1876 1st ed OS 25" sheet.	D
MV098				SO 0380 0557 -	Tramroad/	Early 19th	Haulage tramroad or plateway, approx 670m in length, extending N	C



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
				SO 0385 0623	plateway	century	from the N side of the Nant Cwm Glo stream gorge to join the main haulage route MV101 through Heolgerrig village to the Cyfarthfa cokeyards. The S section of the route as far as the Nant Cwm-pant-bach stream was re-aligned in the late 1850s or early 1860s. Probably constructed in the 1830s or 40s as a main haulage route from Cwm Glo Pit; it appears in the 1850 Tithe map, and (in its remodelled form) in the 1876 1st ed OS 25" sheet. Dismantled by the 1900 2nd ed OS 25", after the disuse of Cwm Glo and Coedcae Pits.	
MV099				SO 0387 0564 - SO 0382 0604	Tramroad	Mid 19th century	Tramroad, approx 470m in length, from Cwm Glo Pit to junction of older haulage tramroad/plateway MV098 just S of Nant Cwm-pant-bach. Well-preserved with sections of stone blocks in situ. Not shown on 1850 Tithe map, but probably in place by the early 1860s, when Coedcae Pit was in use, as a re-alignment of the earlier haulage route from Cwm Glo Pit.	C
MV100			WM32	SO 0400 0459 - SO 0369 0510	Tramroad	Mid 19th century	Tramroad, approx 1.1km in length, from ironstone workings MV103 to the Cwm Glo Pit Incline (MV144) and the main Cyfarthfa haulage route MV099. The S section included several branch tracks to individual ironstone levels and quarries. Incorporated part of the route of the older haulage plateway/tramroad MV101 to S of the Nant Cwm Glo. Crossed the Nant Cwm Glo stream gorge via an embankment (now removed). Not on 1850 Tithe map but probably in place by the 1860s. Shown on 1876 1st Ed OS 25" sheet. Disused by 1900 2nd ed OS 25" sheet.	D
MV101				SO 0402 0531 - SO 0371 0667	Tramroad/ plateway	Early 19th century	Main haulage tramroad or plateway, approx 1.48km in length, extending NW from ironstone level MV078 (2834m), across the Nant Cwm Glo stream gorge at SO 0396 0551 via an embankment (now removed), across the Nant-cwm-pant-bach at SO 0391 0609, through Heolgerrig village to the Cyfarthfa cokeyards. Probably constructed at end of the 18th or beginning of the 19th century, it appears in the 1814 OS surveyors drawing and the 1850 Tithe map. The pronounced curving detour through the Nant Cwm Glo valley was probably an attempt to reduce the gradient of the stream gorge. By the 1876 1st ed OS 25" sheet, the S section of the tramroad as far as its junction with the Cwm Glo pit haulage route had been largely abandoned, apart from two short lengths incorporated into other tramroads. The N section of the tramroad had been dismantled by the 1900 2nd ed OS 25" sheet. Only a few sections of the tramroad have survived subsequent tipping.	B
MV102				SO 0448 0500 -	Branch haulage	19th	Course of possible tramroad extending 300m upslope from Colliers	D

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
				SO 0421 0510	tramroad	century	Row Pit (MV129: 2811m) NW to join possible tramroad MV106. Two or three levels had access to its line. Course shown as a disused track on 1876 1st ed OS 25" sheet, with tramroad still intact along W section towards Colliers Row Pit, and several defunct levels further E. By the 1900 2nd ed OS 25", more of the tramroad to the rear of Lower Colliers Row had been reinstated in order to service a new level. The entire line had been dismantled by the time of the 1919 3rd ed OS 25" sheet. The W section of the line in the vicinity of Lower Colliers Row has been largely obscured by later level tips.	
<b>Archaeological Area 4:</b> outcrop workings on the hillslope on the W edge of the study area.								
MV103	2822m		WM26	SO 0415 0479	Ironstone workings	18th or early 19th century	Extensive length of outcrop workings along the hillslope on the W side of the study area, between the 270m and 300m contours, occupying an area of 700m x 300m. It includes ironstone patch workings with quarried faces and tips and several ironstone levels. These are probably among the earliest workings in the study area, although they continued in use until the later 19th century. The 1850 Tithe map does not show specific workings, but indicates part of the haulage tramroad (MV100), near Pen-y-Coedcae. The 1876 1st Edition OS 25" shows the tramroad extending to the S end of the outcrop workings, with several short linking tramroads and tiplines then in use. Two small buildings are shown on the quarry floors. By the 1900 2nd ed OS 25", all the outcrop workings were in disuse. The presence of mature trees on the remains suggests an early date; there has been some re-use of waste tips to create sheep pens.	B
<b>Archaeological Area 5:</b> fields and scrub woodland with relatively few early industrial workings, lying between Areas 4 and 6.								
MV104	2841m/ 2852m		WM54/ WM66	SO 0424 0495	Coal level	Early 20th century	Level on hillslope with access to trackway or former tramroad MV106. Narrow cutting to NE, 17m in length, with branch track joining trackway MV106. Waste tips extending downslope beyond trackway MV106, to a distance of 90m from level mouth. Not on 1876 1st ed or 1900 2nd ed OS 25" sheets, but shown in disuse in 1919 3rd ed OS sheet. One of several levels adjoining trackway MV106. Note that this is possibly the same level as WM66 (2852m),	D



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							in the original catalogue at SO 0425 0501.	
MV105	2842m		WM55	SO 0439 0480	Coal level	Early 20th century	Coal level adjacent to trackway on hillslope, 150m to SSW of Colliers Row Drift (MV134) Small building 10m to E of level mouth. Tramroad extended N from level mouth to the coal loader MV126 at the base of the Cwm-du Pit Incline. Second tramroad extended E to waste tip, to a distance of 70m from level mouth. Not on 1900 2nd ed OS 25" sheet, but shown on 1919 3rd ed OS 25" sheet. Probably associated with the Colliers Row Drift workings; the modest scale of the waste tips suggests that this may have been a trial level.	D
MV106	2862.0m		WM38	SO 0385 0562 - SO 0440 0483	Tramroad or pack horse route	Late 18th or early 19th century	Line of probable tramroad or pack horse route extending NW along the hillside from the Melin-ganaid road (near Lower Colliers Row) towards Cwm Glo Pit. The NW section of this route is the tramroad MV100. Several levels are sited along its course. Not shown on the 1850 Tithe map, but shown as a track on the 1876 1st ed OS 25" sheet.	E
MV107	2859.0m		WM39	SO 0401 0530 - SO 0429 0460	Droeway or possible pack-horse route	18th/ 19th century	Track along contour of hillside, approx 800m in length, extending SE from the later 19th century haulage tramroad MV100 towards Cwm Pit. Shown on 1st Edition (c. 1838) 1" OS map, and 1876 1st ed OS 25" sheet. The route has probably been altered during the 19th century, but it may have originated as an early droeway or pack-horse track. The S end of the route in the vicinity of Cwm Pit has been covered by waste tips.	D
<p><b>rchaeological Area 6:</b> Canal and Cwm Pit Railway corridor on the E side of the study area rea characterised by substantial terraced embankments, waste tips and frequent structures</p>								
MV108				SO 0410 0580	Cyfarthfa Canal Reservoir	Late 1770s	Sickle-shaped pond on W side of the Cwm Pit Railway, lying to S of the Upper Colliers Row allotments. The pond measured 100m NS x 70m EW. Probably originated as a feeder or possible turning basin for the Cyfarthfa Canal, though it may have been enlarged by the damming action of the railway. The distinctive course of the canal at this point closely mirrors that of the lock 250m to the S; it is possible that the pond had a similar function. The canal joined the pond at its S end at SO 0413 0577; it seems to have left the pond on its E side at SO 0414 0584, at the point where it was later crossed by railway embankment. Shown on the 1850 Tithe map and the 1876 1st ed OS 25" sheet. Now dry and overgrown, though the site remains undeveloped. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	A

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
MV109	2845m		WM58	SO 0423 0436	Cwmfelin Drift	Early 20th century	Colliery Drift mouth, located on hillside 165m to ESE of Cwm Pit main shaft. Part of the Cwm Pit complex. Inclined tramroad extending 125m to WSW to join the Cwm Pit Railway. Colliery waste taken via Cwm Pit incline to extensive tips MV123 to N. Not on 1881 1st ed OS 25", but shown in use on 1919 3rd ed OS 25" sheet. The drift mouth and the W part of the tramroad is located to the S of the study area. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	C
MV110				SO 0423 0570	Ventilation shaft	Later 19th century	Airshaft located on the E side of the Cwm Pit Railway, near Upper Wern. Shown on the 1876 OS 25" sheet, and subsequent OS maps. The airshaft probably lies slightly outside the E edge of the study area.	D
MV111				SO 0428 0541	Cyfarthfa Canal bridge	Late 18th or early 19th century	Footbridge across the Cyfarthfa Canal, located adjoining the canal level between the two Scheduled parts of the canal. Carried a footpath from Pen-cae cottage E to Lower Wern. Footpath across the canal shown on the 1850 Tithe map, and the bridge itself is shown on the 1876 1st ed OS 25". The latter map shows the bridge to have been about 6m in length, and 3.5m in width; it also had an OS benchmark (665.8'). By the 1900 2nd ed OS 25" sheet, the canal to the S of the bridge had been filled in, and the bridge is not shown. The bridge abutments appear to survive, on the N side of the canal level.	A
MV112	2831m		WM44	SO 0429 0541	Cyfarthfa Canal level	19th or early 20th century	Flooded coal level, leading NE directly to the Cyfarthfa Canal. Lies between the two Scheduled portions of the canal, immediately to the S of the canal bridge. Waste tips on the E side of the Cwm Pit Railway. Not shown on the 1876 1st ed OS 25" sheet, but the waste tips do appear on this map. Shown in disuse on the 1919 3rd ed OS 25". Dating uncertain, but it is possible that this is an early canal level which was subsequently re-opened, as was the case with the adjacent Scheduled canal level to the S of this. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	A
MV113	3951m			SO 0430 0427	Cwm Pit Reservoir	Early 19th century	Rectangular pond on hillslope to W of main colliery buildings. Embanked on E side, with sluice. The pond probably supplied the balance hoist for Cwm Pit. Shown on the 1850 Tithe map, the 1876 1st ed OS 25" sheet and subsequent editions. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	B
MV114	2805m		WM8	SO 0432 0517	Reservoir	Later 19th century	Irregular rectangular pond located on N side of the Cwm Glo Incline, 60m W of the Cwm Pit Railway. The pond is aligned NS, 65m x 32m (0.19 ha), terraced into the hillslope, with an embankment on its E	C



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							side. A sluice lies at its SE corner. It was supplied by a leat from the W, extending to the S of Pen-cae Cottage. Not on the 1850 Tithe map, but shown on the 1876 1st ed OS 25" sheet. Its purpose is uncertain, though it may have been provided to supply the Colliers Row Pit balance hoist.	
MV115	2840m		WM53	SO 0433 0504	Coal level	Early 20th century	Coal level located near cottage MV117, adjoining branch haulage tramroad MV102. Short cutting, 10m in length, extending NE. Not on 1876 1st ed OS 25" sheet, but shown on 1900 2nd ed OS 25", with branch tramroads leading to the incline coal loader on the Cwm Pit Railway MV126 (2850m) and to Lower Colliers Drift MV134. Some tips downslope of the branch haulage tramroad, encroaching on the rear gardens of Lower Colliers Row. Shown disused in the 1919 3rd ed OS 25" sheet. Tramlines are no longer visible; level mouth not inspected.	D
MV116	2807m		WM10	SO 0436 0510	Embankment	Later 19th century	Line of an embankment, approximately 90m in length, extending NW from a junction with the branch haulage tramroad MV102 near Lower Colliers Row. Possibly a short length of tramroad. Shown in the 1876 1st ed OS 25" sheet; by the 1900 2nd ed OS 25" sheet, this feature had become encroached upon by allotments belonging to the adjacent Lower Colliers Row.	D
MV117	2810m		WM13	SO 0437 0501	Cottage	Early 19th century	Site of cottage and garden to the rear of (and overlooking) Lower Colliers Row. The cottage measured approx 9m x 6m, set in an enclosure of 0.11 ha. Shown on 1st Edition 1" (c.1838) and Tithe map (1850). Derelict by 1967.	C
MV118	2851m		WM65	SO 0439 0511	Incline structure	Early 20th century	Rectangular building, 8m x 5m, located on S side of the Cwm-du Pit Incline MV145, 25m to NW of Lower Colliers Row. Not part of the original arrangements of the incline; first shown on 1919 3rd ed OS 25" sheet. Possible remains of weighbridge, with iron beam anchor visible, largely buried by tips	C/D
MV119	2806m		WM9	SO 0438 0517	Railway buildings	Late 19th century	Eastern of two small rectangular buildings lying between the reservoir MV114 and the Cwm Pit Railway sidings. The E building measures 7m x 5m. The W building lay 15m to W and measured 8.5m x 4m. Not on 1876 1st ed OS 25", but shown on 1900 2nd ed OS 25". The W building is probably now obscured by tips; the ruins of the E building are visible. The two structures were probably associated with the Cwm Pit Railway sidings, or the adjacent Cwm-du Pit Incline coal loader MV126.	D
MV120	2821m	33,467	WM24	SO 0439 0432	Cwm Pit	Early 19th - early 20th	Early deep shaft colliery on the N side of the Nant Canaid. Position indicated as the terminus of the Cwm Pit Railway MV147 on the 1850	B

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
						century	Tithe map. The 1876 1st ed OS 25" shows the pit in use, with a winding engine house and boiler house to SE of the shaft at SO 0440 0430. Additional buildings were added by 1900 and 1919; the pit continued in use until the 1940s. The surviving structures include the engine house chimney base, kilns, tramway and building foundations. The stack was demolished by 1973.	
MV121				SO 0440 0416	Old Waunwylt farmstead	Post-medieval	Farmstead located on the S side of the Vale of Neath (GWR Merthyr Branch) railway cutting, to SE of the tunnel portal, and about 80m to NE of the modern Waunwylt Farm. Building about 20m x 5.5m, aligned NE-SW, with surrounding enclosures. Shown on the 1850 Tithe map. By the 1876 1st ed OS 25" sheet, the farm had been rebuilt on its present site, and the old building was in ruins. The ruins are shown in the subsequent editions of the OS 25" map.	C
MV122	2804m		WM7	SO 0440 0522	Bridge, Cwm Pit Railway	Early 20th century	Bridge, set in cutting, carrying the Cwm Pit Railway MV147 over the Pen-cae Incline MV146. Not on the 1900 2nd ed OS 25" sheet; incline shown in disuse in the 1919 3rd ed OS 25" sheet.	C
MV123				SO 0440 0555	Cwm Pit late waste tips	Late 19th to early 20th century	Extensive colliery waste tips conveyed by incline on hillside above Cwm Pit. Covers an area of 200m NS x 260m EW. The tips are not shown on the 1876 1st ed OS 25", but were being established by the 1900 2nd ed OS 25", and continued to be tipped until the closure of Cwm Pit in the 1940s.	D
MV124			WM14a (part)	SO 0441 0496	Level	Mid 19th century	Level on hillslope, located 110m to SW of Lower Colliers Row, and 50m W of Colliers Row Drift main portal. Narrow, curving cutting to NE, 30m in length, joining the branch haulage tramroad. Waste tips extending downslope from the far side of the tramroad, to a distance of 75m from the level mouth. Shown, disused and unlabelled, on 1876 1st ed OS 25" sheet. The waste tips were followed, and probably disturbed by a later tramroad, shown in the 1919 3rd ed OS 25" sheet.	D
MV125				SO 0441 0512	Ventilation shaft	Later 19th century	Oval-shaped airshaft located 22m to NW of Lower Colliers Row and 6m SW of the incline coal loader MV126. Shown on 1876 1st ed 25" sheet, possibly still in use. By the 1900 2nd ed OS 25" sheet, it was overlain by a tramroad leading to the incline loader.	D
MV126	2850m		WM64	SO 0442 0512	Incline coal loader	Late 19th century	Coal loading hoist at the base of the Cwm-du Pit Incline MV145, adjoining the Cwm Pit Railway sidings (loading bay MV132) to N of Lower Colliers Row. Narrow rectangular structure, 18m x 5.5m, aligned NE-SW. Not on 1876 1st ed OS 25" but shown on 1900 3rd ed OS 25", serving the incline and level(s) at Lower Colliers Row. Still in use on 1919 3rd ed OS 25" sheet. Footings of small rectangular structure, 2.5m x 4m, visible within area of tipping.	C/D



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
MV127	C09b & C09e (sluice)	91,547		SO 0443 0446	Cyfarthfa Canal feeder pond	Early 19th century	Rectangular pond, located on hillslope 230m to NE of Cwm Pit. The pond is aligned NS and measures 72m x 32 (0.2 ha), with a substantial embankment on its E (downslope) and N sides, and a stone internal lining on these sides. Sluices were provided near its SE corner (C09e) and in its N side. Feeder channels to the canal extended from both ends of the reservoir. The pond is not shown on the 1814 OS surveyors' drawing, but appears on the 1850 Tithe map and the 1876 1st ed OS 25" sheet. It survives largely intact.	A
MV128	2853m		WM67	SO 0443 0470 (approx)	Pit	Probably 19th century	Flooded shallow pit, with associated linear feature (possibly a tramroad), located on the hillside above Cwm Pit, lying downslope of the waste tips. Not shown on any of the OS 25" sheets.	D
MV129	2811m		WM14	SO 0443 0493	Colliers Row Pit	Early 19th century	Colliery pit located near the S end of Lower Colliers Row, and probably directly associated with the cottages. Shown on the 1850 Tithe map, lying immediately to W of the Cyfarthfa Canal, which probably provided access to the cokeyards. The canal feeder lay on the W side of the colliery. The 1876 1st ed OS 25" shows the pit essentially still in its early form, with two small structures, one of which was probably a water balance hoist at the pit head. The partly-dismantled branch haulage tramroad MV102 extended past the pit to the NW, with its replacement line NE to the Cwm Pit Railway, and a third tramroad SE to waste tips. By the 1900 2nd ed OS 25" sheet, the pithead structures had been completely removed, though the shed adjacent to the Cwm Pit Railway had been enlarged (see separate entry). Now overgrown and partly filled in, no remains of associated tramways to level mouths	C
MV130	2809m	19,251	WM12	SO 0443 0506	Lower Colliers Row	Early 19th century	Row of 21 cottages located to the W of the Cwm Pit Railway, to the N of Colliers Row Pit MV129. The row was approximately 77m in length, and aligned NW-SE, with a rear alley and garden plots to front and rear. Individual cottages ranged from 3m to 5m in width, with a possible bakehouse at the N end. Probably not built before 1794, using mixed oak and softwood rafters. Appears to be shown on the 1814 OS surveyors' drawing, and is shown on the 1850 Tithe map and the 1876 1st ed OS 25". By the 1900 2nd ed OS 25", the rear allotments had been encroached upon by waste from an adjacent level, and the allotments had been further reduced and subdivided by the 1919 3rd ed OS 25". The cottages were derelict by 1967 and demolished in 1970. Parts of the ruins are visible.	C
MV131	2843m		WM56	SO 0444 0464	Coal level	Early 20th century	Coal level located on the NE side of Cwm Pit waste tips. Short cutting to E, with waste tips extending downslope to E, to a distance of 70m	D

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							from level mouth. Trackway to NE possibly marks the line of a tramroad to the branch haulage line. Not on 1900 2nd ed OS 25", but shown in disuse on 1919 3rd ed OS 25" sheet.	
MV132	2808m		WM11	SO 0444 0513	Coal loading bay	Late 19th century	Position of coal loading bay adjoining the loading structure MV126, at base of Incline MV145, adjoining the Cwm Pit Railway. On 2nd (1900) and 3rd (1919) ed OS 25" maps.	C
MV133			WM14a (part)	SO 0445 0491	Colliers Row Drift, No 2 Level	Early 20th century	Level located on hillslope, 120m to S of Lower Colliers Row, and 23m to S of Colliers Row Drift main portal MV134. Tramroad extending to NW, taking coal N of Lower Colliers Row to loading structure MV126 on Cwm Pit Railway. Second tramroad extending E to extensive waste tip, 95m x 75m, to a distance of 130m from level mouth, centred on SO 0450 0490. Not on 1900 2nd ed OS 25", but shown in use on 1919 3rd ed OS 25" sheet.	C
MV134			WM14a (part)	SO 0445 0494	Colliers Row Drift, No 1 Level	Early 20th century	Drift main entrance located 95m to S of Lower Colliers Row. Small building near drift portal. Tramroad extending to NW, taking coal N of Lower Colliers Row to loading structure MV126 on Cwm Pit Railway. Waste tips on the site of the old Colliers Row Pit (MV129), to a distance of 80m from the drift mouth. No 2 Level (MV133) was located further upslope, 23m to S. Not on 1900 2nd ed OS 25", but shown in use on 1919 3rd ed OS 25" sheet.	C
MV135				SO 0446 0500	Ironstone level	Mid 19th century	Level located to S of Lower Colliers Row, to W of Colliers Row Pit. Cutting of 40m length, extending W to branch haulage tramroad. Shown disused on 1876 1st ed OS 25" sheet. 1919 3rd ed OS 25" sheet shows site completely obscured by later waste tips. No visible traces surviving.	D
MV136	2830m		WM43	SO 0446 0485	Waste tip	Mid 19th century	Lobed waste tip on hillslope adjacent to trackway, 130m to S of Lower Colliers Row. The tip measures 95m x 50m. Shown on 1876 1st ed OS 25" sheet. This seems likely to have derived from an uncharted level located near the end of trackway MV106, located at approx SO 0440 0482. The 1st, 2nd and 3rd ed OS 25" sheets show a triangulation point on top of the tip, suggesting that the tip was well-consolidated by 1876.	D
MV137	2801m		WM4	SO 0447 0534	Loading Bay	Early 20th century	Disused loading bay on the W side of the Gethin Railway. Concrete shuttered construction. Not shown on 1919 3rd ed OS 25" sheet, but apparently in use in the 1940s.	C
MV138	2820		WM23	SO 0449 0427	Railway bridge	c 1853	Road bridge over the Vale of Neath Railway (GWR Merthyr Branch) and the Nant Canaid stream, located to the SE of Cwm Pit. The bridge is 55m in length and 4m in width. The course of the road leading to the bridge is shown on the 1850 Tithe map, and it was probably	B



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							constructed in time for the opening of the railway in 1853. Shown on the 1876 1st ed OS 25" sheet, and subsequent editions. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	
MV139				SO 0450 0438	Cwm Pit early waste tips	Early to late 19th century	Colliery waste tips, extending for 400m along the N bank of the Nant Canaid, surrounding the site of Cwm Pit. Probably originated in the first part of the 19th century; the 1850 Tithe map describes the stream bank as <i>Caie Pwdwr</i> - spoilt land. By the 1876 1st ed OS 25", the waste tips had extended across all of the available land on the N side of the river. By the end of the century, waste was being conveyed by incline to the hillside above the pit. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	C
MV140	2803m		WM6	SO 0452 0525	Coal loading bay	Early 20th century	Coal loading bay on the Cwm Pit new railway line, at the base of the Pen-cae incline (MV146). Consists of a level platform on the W side of the track, supported by a masonry wall, 18m in width; the incline extends as an embanked track to the base of the wall. Not on the 1900 2nd ed 25" map, though the Cwm Pit new railway was then under construction. Shown OS 25S map and still in use in 1947, redundant by 1951.	C
MV141	2819m		WM22	SO 0454 0433	Railway bridge	c 1853	Small arched stone culvert carrying the Vale of Neath Railway (GWR Merthyr Branch) over Nant Canaid at Cwm Pit. Shown on the 1876 1st ed OS 25". The subsequent branch extension of the GWR Merthyr Branch to Quakers Yard started immediately to the E of the bridge. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	C
MV142	2815m		WM18	SO 0461 0467	Smithy site	Mid to later 19th century	Site of smithy, located on W side of the Cwm Pit Railway, 60m W of Glyndyrys Pit. Not on 1850 Tithe map. The 1876 1st ed OS 25" sheet shows this as a rectangular building, 5m x 4.5m, with a benchmark on its N wall, and a track leading uphill towards the canal feeder channel sluice. It is labelled as a smithy. The building had been removed by the 1900 OS 25" sheet. Now lying in an area of severe flooding. Possibly associated with Glyndyrys Pit.	E
MV143	2818m		WM21	SO 0463 0479	Railway bridge	c 1853	Bridge carrying a footpath over the cutting for the Vale of Neath Railway (GWR Merthyr Branch), adjacent to a bend in the Cyfarthfa Canal. Single span arched masonry bridge, 36m in length across the cutting and 6m width. Not shown on the 1850 Tithe map, but probably constructed as part of the initial works, completed in 1853. Shown on the 1876 1st ed OS 25", and subsequent editions.	B

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
MV144	2857.0m (general)  2827m (masonry footings)		WM31 (general)  WM31a (masonry footings)	SO 0385 0562 - SO 0419 0572	Cwm Glo Pit Incline	Later 19th century	Incline connecting Cwm Glo Pit with the Cwm Pit Railway. Extended from a structure (possibly a brake house) at the incline head, 30m to SW of the pit head, continuing for 390m to the railway. Not shown on the 1850 Tithe map, but shown in use on the 1876 1st ed OS 25" sheet. Disused, and track removed, by the 1900 2nd ed OS 25" sheet. Now a footpath. The remains include short lengths of degraded revetting and a pair of masonry piers on the trackbed MV144 at SO 0388 0561 (2827m: WM31a); the latter feature appears on the 1900 2nd ed OS 25" sheet.	C
MV145	2864.0m		WM35	SO 0380 0534 - SO 0444 0513	Cwm-du Pit Incline	Late 19th century	Incline connecting the tramroad from the Cwm-du and Cornel Waun Pits to the coal loader on the Cwm Pit Railway. Extended SE for 670m on the S side of Pen-cae cottage and reservoir WM8, to the coal loader WM64. Not on the 1876 1st ed OS 25" sheet, but shown in use on the 1900 2nd ed OS 25" and 1919 3rd ed OS 25". Replaced (probably during the 1930s) by the reactivated Pen-cae Incline.	C
MV146	2863.0m		WM34	SO 0382 0535 - SO 0450 0526	Pen-cae Incline	Early 20th century	Incline connecting the tramroad from Cwm-du and Cornel Waun Pits to Cwm Pit and Gethin Railways. Top of incline at junction with earlier Cwm-du Pit Incline WM35 (2864m) near the Pen-y-coedcae cottages. Extended SE for 600m on the N side of Pen-cae cottage, through cutting beneath the Cwm Pit Railway at SO 0440 0522 (2804m), via a level embanked track on a different alignment for 160m to a raised loading bay on the Cwm Pit new railway at SO 0450 0526 (2803m). Not on the 1900 2nd ed OS sheet, but shown disused and partly obscured by tips by 1919. Re-used during the 1930s and still railed in 1947; abandoned by 1951	C
MV147	2858.0m		WM37	SO 0442 0431 - SO 0372 0675	Cwm Pit Railway	Early to mid 19th century	Narrow gauge railway, 2.7km in length, from Cwm Pit to Cyfarthfa Ironworks. Extended from the pit head, following the line of the old Cyfarthfa Canal, crossing Heolgerrig village and continuing to the Cyfarthfa coeyards. It crossed the canal, probably by low bridges, at four points in the study area: SO 0450 0441; SO 04549 0453; SO 0420 0570 and SO 0425 0555. The 1850 Tithe map shows the line extending only as far as Upper Colliers Row, suggesting that it may still have been under construction. Significantly, the railway had a branch line at that early stage, connecting Cwm Pit with the mineral line to Cyfarthfa's sister works at Ynysfach. The Cwm Pit line is shown in its complete form in the 1876 1st ed OS 25", and subsequent editions; the early branch line to the Ynysfach railway had been removed by that stage. The Cwm Pit line was intended to replace the ageing Cyfarthfa Canal as the main haulage route for coal and iron,	B



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							and much of the subsequent industrial development of the study area was based on or around it. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	
MV148	2412m (general)	232,778 (general)	WM1	SO 0422 0549 - SO 0427 0543	Cyfarthfa Canal (Scheduled section A)	Late 1770s	Early tub-boat canal, constructed by Anthony Bacon in the late 1770s, extending for two miles from the Nant Canaid brook to Cyfarthfa Ironworks. Passes several coal levels which were connected to the canal by tunnels. Disused by about 1835-40. Two sections are Scheduled monuments; this is the northern section - SAM Gm467. This part includes a length of about 78m of the canal, with the banks on either side, extending N as far as the culverted section of the canal. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH1.	A
MV149	2412m (general)	232,778 (general) and 93,281		SO 0430 0501 - SO 0432 0535	Cyfarthfa Canal (Scheduled section B)	Late 18th to early 20th centuries	The southern of the two Scheduled sections of the Cyfarthfa Canal, part of SAM Gm467. This part includes a 55m section of the canal, which forms the E edge of the Scheduled area, and an adjacent level mouth and cutting lying to the W of the canal. The level mouth is at SO 0429 0536. It appears in a disused state on the 1876 1st ed OS 25" sheet, with a narrow cutting and a watercourse issuing from the mouth. Shallow waste tips lie on the opposite side of the canal and the Cwm Pit Railway; the tips probably pre-dated the construction of the railway. By the 1900 2nd ed OS 25" sheet, the level had been re-opened; a tramroad extended S along the line of the canal to the coal loader on the Cwm Pit Railway, and a second tramroad carried waste under the Cwm Pit line, via a tunnel. By the 1919 3rd ed OS 25", the level was still in use; the cutting had been widened, and a small building had been erected on the N side of the entrance. On the hillslope above the level, at a distance of 30m from the level mouth, two other structures had been built, probably including the surviving ventilation shaft. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH1.	A
MV150	2867.0m		WM41	SO 0400 0436 - SO 0406 0479	Early farm road and possible pack horse route	Post-medieval	Trackway, 1.2km in length, from the 17th century mill at Melin-ganaid in the Nant Canaid valley, NE across the hillside to Old Rhyd-y-car. Passed to the S of Colliers Row Pit, and crossed the Cyfarthfa Canal by a bridge at SO 0448 0496. The LNWR/B&M Railway crossed the trackway via a bridge at SO 0474 0521. Shown on the 1814 OS surveyors' drawing, the 1850 Tithe map and the 1876 1st ed OS 25" sheet.	E

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
MV151				SO 0432 0537 - SO 0443 0512	Tramroad	Early 20th century	Tramroad along the W side of the Cwm Pit Railway, connecting the Cyfarthfa Canal Level (SAM 467g) with the railway coal loader at the base of the Cwm-du Pit Incline. Extended SSE for 275m, parallel with the railway embankment, partly encroaching on the line of the old Cyfarthfa Canal, passing beneath the Pen-cae Incline via a bridge at SO 0437 0522. Not on the 1876 1st ed OS 25" sheet, but shown in use on the 1900 2nd ed and 1919 3rd ed OS 25" sheets.	D
MV152	2865.0m		WM1b	SO 0452 0457 SO 0450 0496 -	Cyfarthfa Canal feeder channel	Early 19th century	Feeder channel for the Cyfarthfa Canal. Extended N for a distance of 460m, roughly parallel with the line of the canal itself, from the canal reservoir to Colliers Row Pit. The feeder was laid along the contour of the hillslope, with a substantial embankment on its E (downslope) side. A sluice at SO 0456 0465 marked the position of a leat feeding to the canal (subsequently culverted). The feeder channel is not shown on the 1814 OS surveyors' drawing (though this plan does not show all such features), but it appears on the 1850 Tithe map and the 1876 1st ed OS 25" sheet. By the 1900 2nd ed OS 25", the channel had become greatly swollen to N of the sluice, and a small building had been erected on the bank at SO 0457 0470. A second sluice is shown at this date at SO 0452 0488. By the 1919 3rd ed OS 25" sheet, the N part of the channel had been covered by waste from the Colliers Row Drift levels. The S part of the channel and the reservoir survives largely intact. This feeder system may be an addition of the end of the 18th or early 19th century, rather than part of the original canal, though its date is uncertain. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	A
MV153	2861.0m		WM25	SO 0435 0430 - SO 0435 0456	Cwm Pit Incline	Late 19th century	Incline, 290m in length, conveying colliery waste N from the pit head at Cwm Pit, via a bridge over the Cwm Pit Railway, to extensive tips on the hillside above. Intact brick-lined arched opening into hillslope at base of incline, with rail trackbed extending NE, beyond main colliery buildings. Not on 1876 1st ed OS 25" sheet (when waste was still being tipped along the Nant Canaid), but shown in use on the 1900 and 1919 editions. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	A
MV154	2866.0m		WM1c	SO 0457 0455 - SO 0462 0480	Cyfarthfa Canal (Cwm Woods section)	1770s	Section of the Cyfarthfa Canal at Glyndyrys, adjacent to the embanked feeder channel and reservoir, further up the hillslope. This section, of some 250m length, is one of the longest portions of the canal to remain still uncovered by railways and tips, though the area is heavily	A



Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							overgrown. This section has been recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	
<b>Archaeological Area 7: GWR and Gethin Railway corridor</b> Area characterised by substantial terraced embankments, with much recent reclamation								
MV155				SO 0434 0550	Weir	Later 19th century	Masonry weir on the Nant Llwyn-yr-eos/ Nant Rhyd-y-car stream, between the Cwm Pit and Gethin Railways. The weir was approximately 18m across; a sluice on the S bank of the stream conveyed water to an embanked leat. The leat and its pond are not shown on the 1850 Tithe map, but appear on the 1876 1st ed OS 25" sheet; the leat was still intact in the 1919 3rd ed OS 25" sheet. Now largely destroyed.	D
MV156				SO 0436 0545	Coal drift	Early 20th century	Coal level (drift), located between the Cwm Pit and Gethin Railways, 30m to S of the Nant Rhyd-y-car. Tramroad to NE, conveying waste to a tip along the W side of the Gethin Railway, to distance of 70m from the level mouth. A branch line connected this with the Cyfarthfa Canal level tramroad, 120m to SW. Not on the 1900 2nd ed OS 25" sheet, but shown in use on the 1919 3rd ed OS 25" sheet.	D
MV157	2800m		WM3	SO 0438 0542	Brick-lined pit	Early 20th century	Brick-lined pit, located between the Cwm Pit and Gethin Railways, on the N side of the waste tips from the Cyfarthfa Canal level. Not shown on the 1919 OS 25" sheet, and possibly dates to the 1940s.	D
MV158	2802m		WM5	SO 0453 0530	Reservoir	Later 19th century	Irregular rectangular pond, located on the W side of the B&M/LNWR Railway. The pond is 55m x 22m (0.12 ha), embanked on E side, with a sluice at N end. Supplied from the NW by an embanked leat, extending for some 290m from a weir on the Nant Rhyd-y-car at SO 0434 0550 (see separate listing). Not on the 1850 Tithe map, but shown on the 1876 1st ed OS 25". Its purpose is uncertain; it may have been provided as a feeder for the balance hoist at Rhyd-y-car Pit, 130m to NE, though the pit itself is earlier. The pond is intact, though most of the supply leat has been covered.	C
MV159	2816m		WM19	SO 0467 0467	Glyndyrys Pit	c 1850	Coal and ironstone pit, located between the Cwm Pit and Gethin Railways, 150m to W of Glyndyrys Pond. Not marked on the 1850 Tithe map, but sunk at about that time, initially for ironstone extraction (the Tithe map description of the land as <i>Caie Pwdwr</i> - spoilt land - implies that it was already being tipped upon). Underground explosion recorded in May 1852 (Thomas 1981, 318). Shown on 1876 1st ed OS 25", with two small buildings (probably	C

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							including a balance hoist headgear), surrounded by level waste tips, distributed by parallel tramroads, and extending N to Lower Colliers Row, at a distance of 430m from the pit head. Water for the balance hoist may have been supplied from the adjacent Cyfarthfa Canal. By the 1900 2nd ed OS 25", the pit was disused, and the Cwm Pit new railway was being laid across the waste tips to N of the pit. Area around pit (marked by iron pipe) has been partly landscaped. No remains now visible above ground.	
MV160	2814m		WM17	SO 0464 0475	Cottage	18th or early 19th century	Cottage located 80m NW of Glyndyrys Pit, adjoining the E side of the Cwm Pit to Ynysfach branch tramroad. Cottage was aligned NNW-SSE, approx 8.5m x 6.5m, set in an enclosure of about half an acre. Shown on the 1850 Tithe map. By the 1876 1st ed OS 25", the cottage was in ruins, and the enclosure was becoming encroached by waste tips from Glyndyrys Pit. By 1900, the site had been partly covered by the embankment for the Cwm Pit new railway. No remains now visible.	D
MV161	2812m		WM15	SO 0469 0498	Signal Box	Early 20th century	Signal box positioned on platform between the Ynysfach and Gethin Railways, to N of their junction. Not on 1900 2nd ed OS 25", but shown in use on the 1919 3rd ed OS 25". Now partly standing. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	C
MV162	2813m		WM16	SO 0465 0506	Ironstone level	Mid to later 19th century	Ironstone level, located at the N end of the platform between the Ynysfach and Gethin Railways, 80m to NW of the later signal box ruins. Shown on the 1876 1st ed OS 25" sheet, and subsequent editions. Now flooded and partly filled-in.	D
MV163				SO 0475 0490	Waste tip	Late 19th to early 20th century	Extensive linear waste tip extending for 390m along the E side of the Ynys-fach Railway from SO 0470 0514 to SO 0480 0475. Not on the 1876 1st ed OS 25". The 1900 2nd OS 25" shows the tip in an early form, encroaching on the fields beside the railway. Small building between the Ynys-fach line and the tipping tramroad at SO 0469 0509. By 1919, the tip had been widened.	D
MV164	2817m		WM20	SO 0479 0468	Cyfarthfa Crossing	c 1853	Junction of the Vale of Neath Railway (subsequently the GWR Merthyr Branch) with the Gethin Railway and the Ynys-fach Railway, to the W of Glyndyrys Pond. Surviving iron girder bridge carries the Vale of Neath Railway (PRN 2500m) over the Ynys-fach line and other bridge abutments. Remains of the signal box at SO 0479 0465. Shown on the 1876 1st ed OS 25" sheet. By the 1900 2nd ed OS 25", the junction had become more complex with doubling of tracks and the extension of the GWR Merthyr Branch S to Quakers' Yard.	B

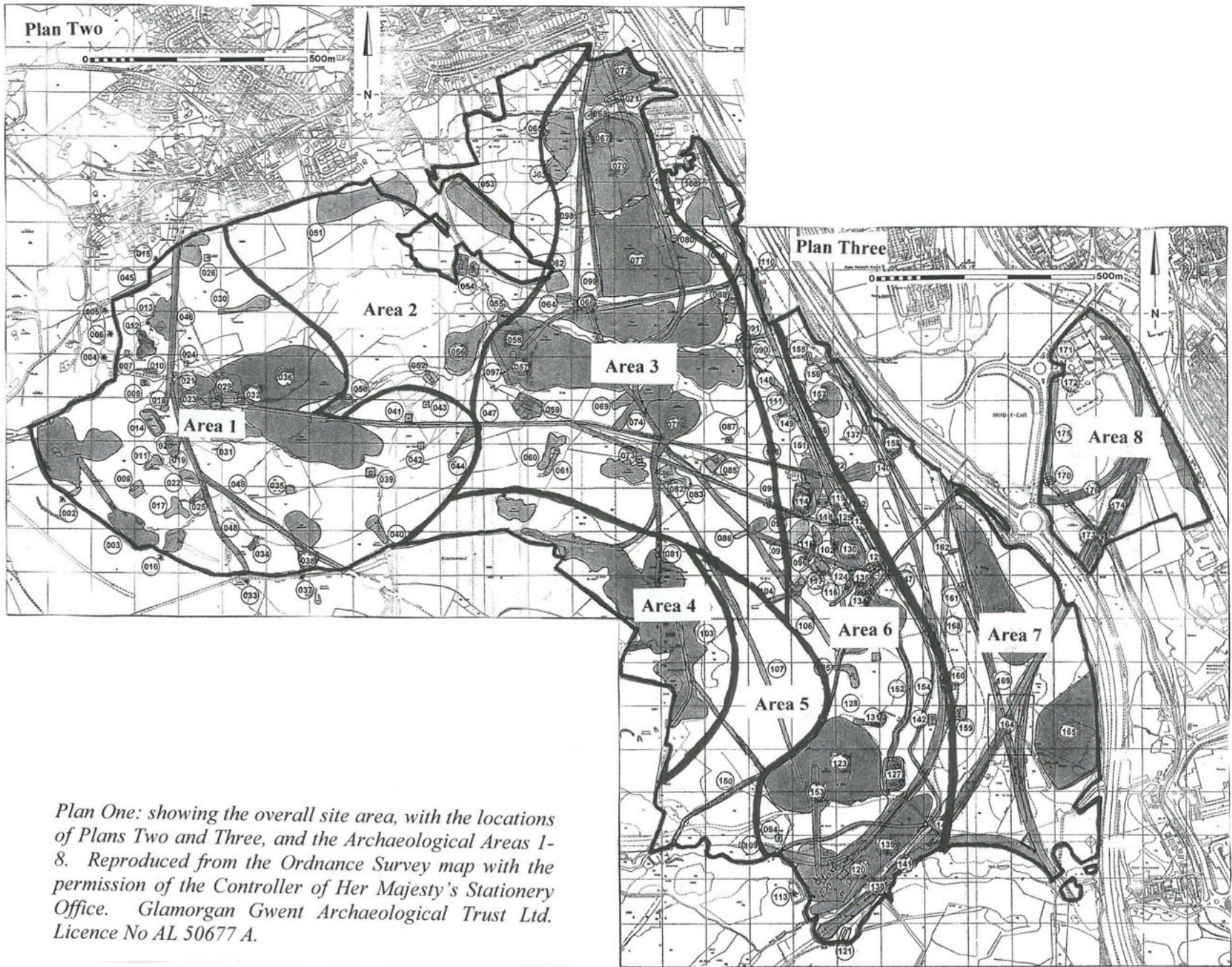


Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
							Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	
MV165	2518m and 2190m	85,180		SO 0500 0470	Glyndyrys Pond	c 1806	Supply reservoir for the Glamorganshire Canal. Large, irregular rectangular pond, embanked on its W and S sides. Shown on the 1814 OS surveyors' drawing, the 1850 Tithe map and subsequent OS maps. Destroyed on its E side by A470 trunk road extension, though part of the SW section of the embankment remains.	C
MV166	2463m			SO 0387 0641 - SO 0634 0288	Gethin Railway	c 1860	Tramroad (subsequently a mineral railway) from the Gethin Collieries to Cyfarthfa Ironworks. Not shown on the 1850 Tithe map; probably constructed soon after the extension of the Gethin No 2 Pit in about 1860. Shown on the 1876 1st ed OS 25", and subsequent editions, with the LNWR/B&M Joint Line running alongside it for part of its length. Part of the trackbed survives within the study area, including the masonry abutments for a bridge carrying the railway over the footpath to Rhyd-y-car at SO 0460 0511. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	B
MV167			WM33	SO 0396 0550 - SO 0415 0565	Tramroad	Later 19th century	Tramroad extending NE for 330m from coal level on the S side of the Nant Cwm Glo to the Cwm Pit Railway. The initial 160m follows the line of the defunct older main haulage route MV101 leading through Heolgerrig to the Cyfarthfa works. From that point, the tramroad turns to the NE, running almost parallel with the Cwm Glo Pit Incline, reaching the Cwm Pit Railway to the N of Upper Wern. Not on the 1850 Tithe map, but shown in use in the 1876 1st ed OS 25" sheet. The tramroad had been removed by the 1900 2nd ed OS 25".	D
MV168				SO 0463 0476 - SO 0465 0517	Cwm Pit to Ynys-fach branch tramroad	Early 19th century	Tramroad extending NE for a distance of 390m from the Cwm Pit Railway near Glyndyrys to the Ynys-fach Railway near Rhyd-y-car. The tramroad was one of two original branches of the Cwm Pit Railway, and appears to have been completed earlier than the route to the Cyfarthfa works. Shown on the 1850 Tithe map, probably then in use. By the 1876 1st ed OS 25", the branch tramroad had been removed and its site was largely occupied by waste tips from Glyndyrys Pit. A short section of tramline remained in place with its trackbed near the disused ironstone mine close to the Ynys-fach junction. This section of line was relaid by the time of the 1900 2nd ed OS 25" sheet, connecting the Gethin and Ynys-fach Railways, and was still in place in 1919. The N section of the trackbed is visible for a distance of 100m to SW of the Ynys-fach junction.	D
MV169	D38m			SO 0513 0440 -	Ynys-fach	Early 19th	Tramroad (subsequently a narrow gauge railway) from Abercanaid Pit	B

Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
				SO 0447 0590	Railway	century	to the Ynys-fach Ironworks. The tramroad passed to the S and E of the Glyndyrys Pond, continuing N on a straight course to the ironworks coke yards, for an overall distance of 1.75km. Shown on the 1850 Tithe map with the branch line from the Cwm Pit Railway. By the 1876 the Cwm Pit branch line had been removed, the track had been crossed by the Vale of Neath Railway (GWR Merthyr line) and had been doubled in places. By the 1900 2nd ed OS 25", the ironworks had closed, but the railway was still apparently in use, serving the Ynys-fach colliery. It was shown still railed on the 1919 3rd ed OS 25" sheet. The railway embankment survives within the study area. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	
<b>Archaeological Area 8: East Rhyd-y-Car</b> Detached parcel of land to E of the Glamorganshire Canal								
MV170		43,141		SO 0489 0521	Canal bridge (site)	c 1800	Site of early cast iron road bridge carrying a road over the Glamorganshire canal, located to the SW of Old Rhyd-y-car farmstead. The bridge has been moved in recent years to Chapel Row. It appears on the 1850 Tithe map, the 1876 1st ed OS 25" sheet, and subsequent editions. Possible remains of abutments.	C
MV171		91,555		SO 0490 0551	Canal bridge	c 1794	Remains of stone-arched bridge carrying access road over the Glamorganshire Canal. The bridge is shown on the 1850 Tithe map, the 1876 1st ed OS 25" sheet, and subsequent editions. Uncertain whether this site lies within the study area.	A
MV172				SO 0491-0545	Canal aqueduct	1794	Site of aqueduct carrying the Glamorganshire Canal across the Nant Rhyd-y-Car. Shown in the 1852 Tithe map, the 1876 1st ed OS 25" and subsequent editions. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4. The present condition of the aqueduct is uncertain; its W side is covered by the modern Rhyd-y-Car roundabout.	A
MV173	2425m	96,558		SO 0501 0509	Rhyd-y-Car skew bridge	1849-53	Stone and brick skew bridge carrying the Vale of Neath (GWR Merthyr Branch) Railway over the Glamorganshire Canal. Three-arched bridge of sandstone and brick, possibly designed by Brunel. Grade II Listed Building. Shown in the 1852 Tithe map, the 1876 1st ed OS 25" and subsequent editions. Recognised as a feature of local heritage interest in the Merthyr Tydfil Borough Local Plan (Deposit 1996), Schedule BH4.	A
MV174	2500m			SN 8913 0648 -	Vale of Neath	1853	Embankment for railway line. The course of the line under	C

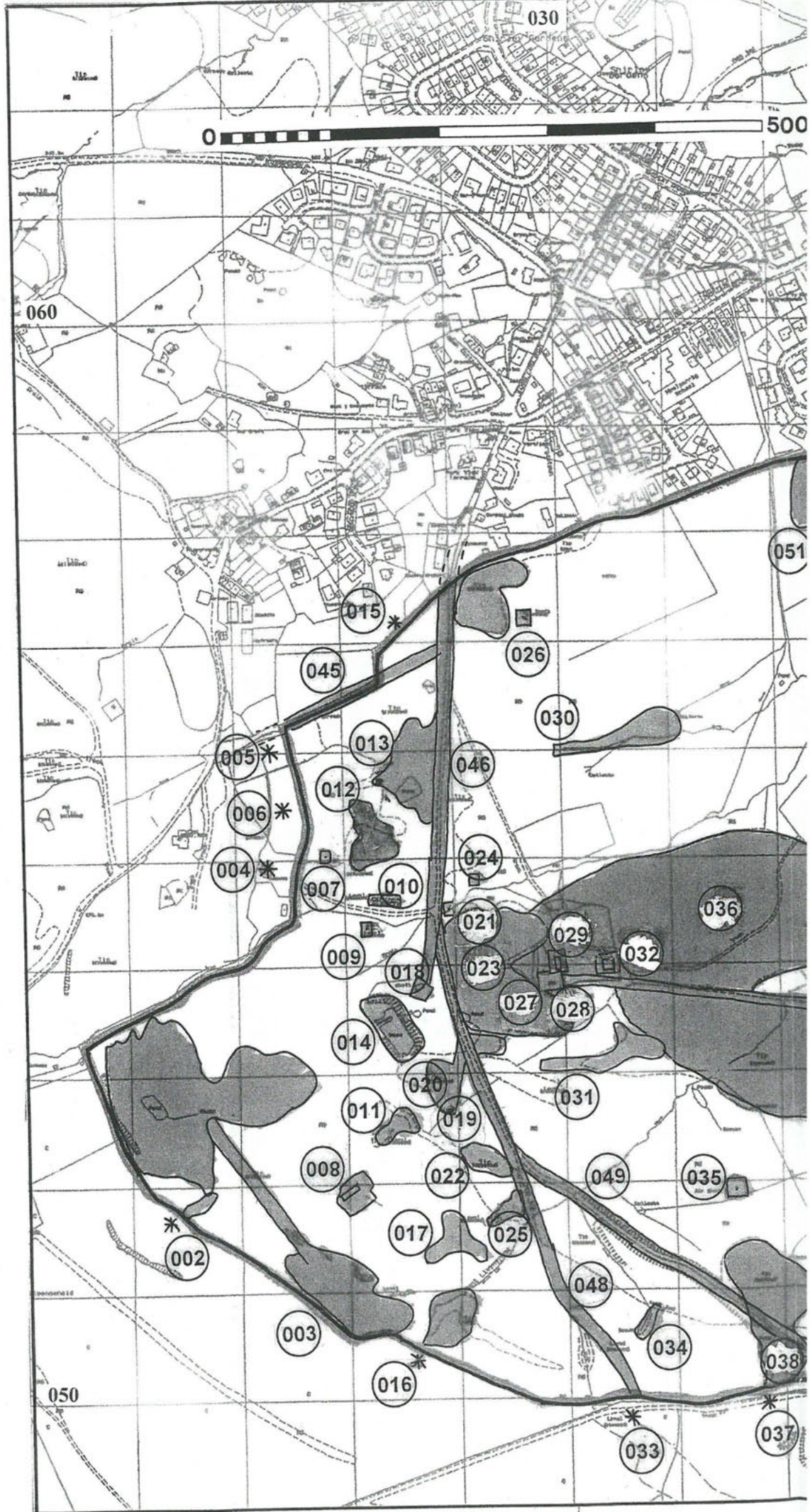


Project No (MV)	PRN	NPRN	Old No (WM)	NGR	Type/Name	Date	Description	Grade
	D37			SO 0500 0606	Railway (GWR Merthyr Branch)		construction is shown on the 1850 Tithe map; the railway appears on the 1876 1st ed OS 25", and later editions. Crossed the Nant Rhyd-y-car at SO 0508 0524.	
MV175				SO 0492 0510 - SO 0502 0507	Glamorganshire Canal	1794	Part of the course of the Glamorganshire Canal (c 500m length). This section, adjoining the modern road across Rhyd-y-Car, has been largely infilled. Includes the site of the cast iron footbridge, now relocated (MV173), the site of a stone-arched footbridge (MV171) and the aqueduct across the Rhyd-y-Car stream (MV172).	A
MV176				SO 0494 0516 - SO 0495 0580	B&M/LNWR Joint Line	1868	Embankment for railway line across the Taff valley. Shown on the 1876 1st ed OS 25" sheet, and subsequent editions. Part survives within Area 8.	C



*Plan One: showing the overall site area, with the locations of Plans Two and Three, and the Archaeological Areas 1-8. Reproduced from the Ordnance Survey map with the permission of the Controller of Her Majesty's Stationery Office. Glamorgan Gwent Archaeological Trust Ltd. Licence No AL 50677 A.*





Plan Two: showing the west side of the study area. Individual man with the permission of the Controller of Her Majesty's St



