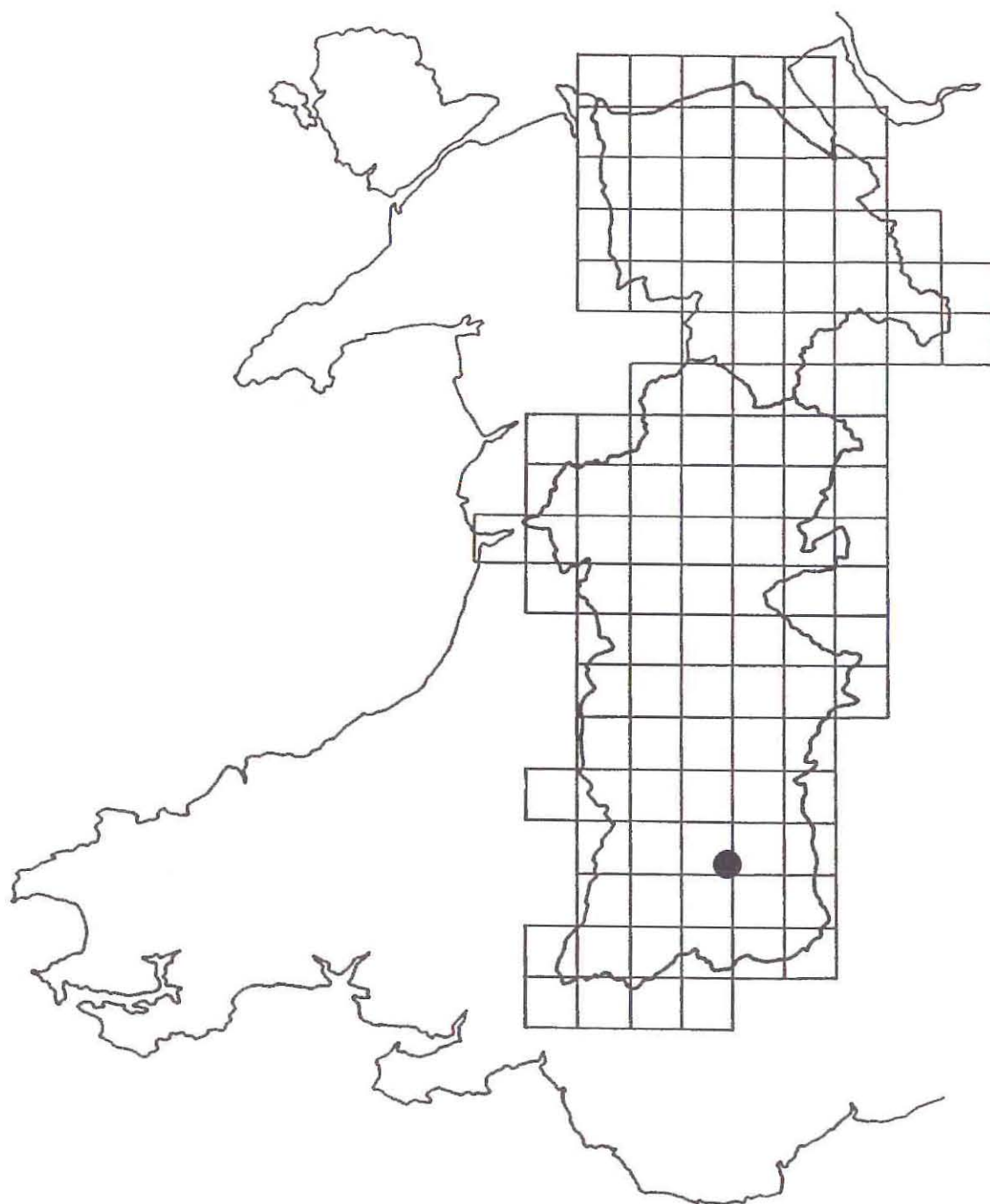


*Proposed Felinfach Bypass, near Brecon, Powys*

FIELD EVALUATION



*Proposed Felinfach Bypass, near Brecon, Powys*

**FIELD EVALUATION**

**by R Hankinson**  
May 1995

Report prepared for: Welsh Office Highways Directorate

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## CPAT Report Record

### Report and status

CPAT Report Title	PROPOSED FELINFACH BYPASS, NEAR BRECON, POWYS		
CPAT Project Name	FELINFACH		
CPAT Project No	615 & 623	CPAT Report No	141
Confidential (yes/no)	No	draft/final	FINAL

### Internal control

	name	signature	date
prepared by	R. HANKINSON	ppd Jones	19/9/95
checked by			
approved by	A. M. GIBSON		19/9/95

### Revisions

no	date	made by	checked by	approved by
APPENDIX 1 & 2	19/9/95	R. HANKINSON	N. W. JONES	A. M. GIBSON

### Internal memo


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## SUMMARY

A combined desk-top and fieldwalk survey was carried out on the line of the proposed A470 Felinfach bypass, near Brecon, Powys, for the Welsh Office Highways Directorate.

A number of sites were identified in the area affected by the proposal, one of which, a possible Roman road, merited archaeological excavation prior to the commencement of site works. A system of drainage management features were identified in the proposal area, which required a measured survey.

As a second stage to the proposal, a section was excavated across the supposed Roman road and measured surveys were conducted of the Roman road and drainage system, the results of which have been appended to this report as Appendices 2-4.

## **1 Introduction**

- 1.1 In February 1995, Clwyd-Powys Archaeological Trust (CPAT) Contracting was asked by the Welsh Office Highways Directorate to provide a quotation for a combined desk-top study and fieldwalk survey on the line of the proposed A470 Felinfach Bypass, situated 5km to the north-east of Brecon in Powys.
- 1.2 The quotation was accepted by the Welsh Office Highways Directorate in March 1995. The desk-top and fieldwalk elements of the programme of work were carried out in May 1995, and this report written immediately afterwards.

## **2 Geographical Background**

- 2.1 The area of the proposed Felinfach Bypass follows a curving line from SO 0880 3220 to SO 0983 3348, a total distance of approximately 1.7km; it is located immediately to the south-east of the village and the present line of the A470 (See Fig 1).
- 2.2 The southern end of the proposed route diverges from the present line of the A470 at the house called "Lower Penishawain", approximately 1km to the south-south-west of Felinfach. From there it follows a broad north-east facing spur, before descending into the valley of the Afon Dulas. After crossing the river and its tributaries, the route travels along the valley floor in a north-easterly direction to join up with the present line of the A470, approximately 0.7km north-east of the village of Felinfach.
- 2.3 The solid geology of the area consists of sandstone likely to be of Devonian age (Pringle and George 1948). The soils derived from this underlying geology are well drained fine loamy reddish soils of the Milford Soil Association (Rudeforth et al 1984).

## **3 The Desk-top Study (Stage 1)**

- 3.1 Readily available documentary, cartographic and aerial photographic sources were consulted. The detailed references to these sources can be found in Section 9, however, a short synopsis is included here to inform of the repositories accessed.
- 3.2 The Regional Sites and Monuments Record (SMR) held by CPAT contains a record of all known archaeological sites. All sites which had been previously recorded in the proposal area are identified by their Primary Record Number (PRN) or Roman Road number (RR) in Section 6. Sites which have been recognised as a result of this scheme of work are given a site number in Section 6.
- 3.3 A number of documentary and cartographic sources relating to the area are held by the National Library of Wales, Aberystwyth (NLW). In addition, the Powys County Archive in Llandrindod Wells was examined, but no information specific to the proposal area was discovered.

- 3.4 Two collections of vertical aerial photographs were examined. These collections, dating from 1946-60 and 1972, produced respectively by the RAF and the Ordnance Survey, are contained in the aerial photographic collection of the National Monument Record (NMR) of the Royal Commission on Ancient and Historic Monuments in Wales (RCAHMW), Aberystwyth.
- 3.5 The results of the desk-top study (Stage 1) are combined with those of the fieldwalk survey (Stage 2) in section 5, below.

#### **4 The Fieldwalk Survey (Stage 2)**

- 4.1 Access was obtained to the fields on the route of the bypass which were examined by transect field walking techniques, modified to suit the local field pattern. A transect width of approximately 30m was adopted.
- 4.2 The primary role of the field survey was to evaluate the nature of the sites identified by the desk-top study, which were examined for surviving features. In addition, a search was made for previously unrecorded sites; any extant sites discovered were described, measured, photographed and sketched as appropriate.
- 4.3 The results of the fieldwalk survey (Stage 2) are combined with those of the desk-top study (Stage 1) in section 6, below.

#### **5 Archaeological Background**

- 5.1 The archaeological background to the area is dealt with by period, and is based on the local information held in the SMR. Extracts from the Regional SMR are the copyright of CPAT and reproduced here by permission. Reference has also been made to published works.
- 5.2 The locality was certainly occupied during the prehistoric period, as evidenced by Bronze Age funerary and ritual monuments (eg PRNs 5605 and 6761). Later prehistoric activity is represented by the Iron Age hillfort of Tre Durn (PRN 547), located c.2km to the north-east of Felinfach. It is worthy of note that the evidence for prehistoric activity in the immediate locality is fairly sparse in relation to that for the medieval and post-medieval periods; a possible explanation of this may be that farming activity, post-dating the Norman conquest of the area, has obliterated the surface traces of prehistoric sites. If this is the case, then it is probable that sub-surface traces of these putative prehistoric sites still remain to be discovered in the area.
- 5.3 Evidence of Roman activity in the locality is provided by the Roman road (RR 63b) from Brecon to Kenchester. The line of the road which is given by the sources consulted travels north-eastward from the fort of Y Gaer, near Brecon, crosses the line of the proposed bypass, and joins the present A470 at Felinfach, following the modern road line thereafter. No evidence of Roman habitation sites in the vicinity of the proposal have yet been discovered.

- 5.4 Evidence of significant levels of medieval occupation has been recognised within the vicinity of the proposal area. A survey of historic settlements in Brecknock Borough (Martin and Walters 1993), while suggesting that the village of Felinfach is of fairly recent origin, mentions two references to medieval land holdings in its vicinity: In 1322, St Peter's Abbey, Gloucester is said to have owned lands at 'Tulecrin' (Tylecrwn), and in 1380 the Earl of Hereford, Humphrey de Bohun, owned a cottage and four acres of the same name. The occupation of the area in the medieval period may be related to the ecclesiastical land holding (Martin and Walters 1993), which would presumably have been a result of the Norman occupation of the area, perhaps as early as the 11th century; in 1093 Rhys ap Tewdwr, king of South Wales, is said to have been killed by "Frenchmen who were inhabiting Brycheiniog" (Jones 1952), which is equivalent to the former county of Brecknock, in which the proposed development lies. In corroboration of the documentary evidence, a number of medieval moated sites occur in the immediate area. These include the scheduled site, PRN 543 (SAM B49), c.1km to the north-east of Felinfach, which is in near the north-eastern terminal of the proposed bypass, although outside the affected area.
- 5.5 Evidence for post-medieval occupation in the vicinity of the proposal area is provided by five farmhouses, of longhouse or longhouse derivative type, such as Tyle Crwn Uchaf (PRN 6955) and Tyle Crwn Isaf (PRN 6956); the initial phase of one of these houses, Llwyn Cynog (PRN 6961), has been dated to the end of the medieval period. Dwellings such as these demonstrate the predominance of agricultural activity during the period, and suggest that it had continued from medieval times. Two watermills, related to local arable agriculture, have been recorded in the immediate vicinity of the proposal, Felinfach Mill (PRN 23022) and Felin Cwm Mill (PRN 6736). These sites are considered to date to the post-medieval period, although it is possible that they also had a medieval origin. The other building in Felinfach which is thought to belong to this period is the 18th century Griffin Inn (PRN 2698), a listed building which may be related to the turnpike road, probably constructed in the later 18th century, from Brecon to Hay-on-Wye.
- 5.6 The settlement of Felinfach appears to be relatively modern in date, and apart from its foundation, which probably occurred during the 19th century (Martin and Walters 1993), there appears to have been little change in the predominance of agricultural and related land usage in the area.

## 6 Results of the Desk-top Study and Fieldwalk Survey

6.1 All sites which are known in the proposal area and its immediate surroundings at present are listed below in the form of a gazetteer. They are arranged in order of Primary Record Number (PRN), Roman Road number (RR), and site number, with site number signifying that the site has been discovered as a result of this assessment. Following the location and descriptive data for each site is a highlighted category designation. The categories assigned follow the guidelines set out in section 4.7.3 of Cadw's draft Archaeology and the Trunk Road Programme in Wales: a Manual of Best Practice, and are described below:-

Category A - Sites designated as being of national importance which meet the criteria for scheduling or listing. It is presumed that sites in this category will be preserved and protected in situ.

Category B - Sites of regional or county importance which do not meet the criteria for scheduling or listing, but which are nevertheless of particular importance within the region. Preservation in situ is the preferred option for these sites, but if loss or damage is unavoidable, appropriate detailed recording will be undertaken.

Category C - Sites of district or local importance which are not of sufficient importance to justify preservation if threatened, but which merit adequate recording in advance of loss or damage.

Category D - Minor and damaged sites which do not merit inclusion in a higher category, for which rapid recording should be sufficient.

Category E - Sites whose importance could not be fully determined from the desk-top assessment and field search. These sites are to be subject to evaluation in Stage 3, before they can be assigned to Categories A-D.

**PRN 543 (SAM B49) Moated Site** Located at: SO 1003 3350

A medieval moated site of rectangular plan, measuring approximately 80m north-east/south-west by 70m north-west/south-east. The site is still visible as a series of earthworks, centred at the location given above. The site is statutorily protected.

**Category A**

**PRN 2698 Griffin Inn** Located at: SO 0916 3320

This 18th century inn is alongside the present A470, and may well have been built as a result of the construction of the Brecon to Hay-on-Wye turnpike road, probably in the later 18th century. The stone built part of the adjacent farm buildings was probably contemporary; it may have been used as stables associated with the inn.

**Category A**

**PRN 6736 Felin Cwm Mill** Located at: SO 0894 3260

The earliest reference to this watermill found, is in a document dated September 1754. This refers to "...the water corn grist mill Mellin y Cwmb, with a parcel thereto adjoining and belonging called Gwar y Cwmb", which was described as being in an abstract of a deed of March 1735. The mill is named "Felin y Cwm" on the Ordnance Surveyors drawing of 1814, which is the first cartographic evidence of its existence. The tithe map for Llanfilo of 1841 suggests that the water system for the mill involved the splitting of the stream above the waterfall to its south; the eastern channel of the stream was then used by the mill, before joining up with the western channel immediately downstream of the mill. The eastern channel had disappeared on a map surveyed in 1903, which suggests that the mill was disused at that time.

The evidence so far discovered confirms that the mill was extant in the post-medieval period. A medieval date for its construction cannot be ruled out; Bowen (1987) gives late medieval dates for two mills in Breconshire referred to as "grist mills", designed primarily for grinding oats and barley.

**Category B**

**PRN 6790 Felin Cwm Hollow-way** Located at: SO 0886 3255 to SO 0894 3260

A sunken trackway, c.5m in total width, leading from the vicinity of Felin Cwm, westward, which was first depicted cartographically in 1814. The trackway may have been related to the mill (PRN 6736), although with the mill being located below the waterfall above which the trackway ends, access would undoubtedly have proved difficult. It may therefore be possible that the mill was accessed from the stream valley below, with the trackway being used to provide access to fields on the opposite (eastern) side of the stream.

**Category C**

**PRN 23022 Felinfach Mill** Located at: SO 0911 3324

This mill, from which the settlement takes its name, is of three storeys and has an associated water system (Bowen 1987). It has recently been converted into a dwelling, but has been ascribed a post-medieval date; the 1765 perambulation of Talach-ddu parish is described as starting at "Glasey Velin Vach", which appears to signify that the mill was extant at that time.

**Category B**

**RR 63b Roman road from Brecon to Kenchester** Located at: SO 0885 3256

This road, from the fort of Y Gaer near Brecon, is thought to take the line depicted on Fig 2, as described in Margary (1973). A road on this line shows up as a cropmark on an RAF aerial photograph taken in May 1946, and at the point where it is crossed by the proposed bypass it is visible as a slight, but readily discernible hollow, approximately 8m in total width, alongside the modern hedgerow.

Evidence from cartographic sources confirms that no road has followed this line from 1814 to the present day. A map of 1781 may possibly suggest that it was the turnpike road at that time, although the interpretation of the map is difficult owing to its relative lack of

accuracy. It should be noted that the possible use of this line by a turnpike road does not necessarily preclude the road from having a Roman origin.

**Category E**

**SITE 1 Lower Penishawain House** Located at: SO 0882 3221

This house is located immediately adjacent to the south-western end of the proposed bypass. A house appears to be depicted at this location on the map dated 1781, mentioned above, but whether this is incorporated in the present house has not yet been determined.

**Category B**

**SITE 2 Quarry** Located at: SO 0890 3267

A linear quarry, measuring c. 120m north-south, located on the upper east-facing side of the valley containing Cwm Felin Mill. The material quarried is sandstone, which would have been used for local building purposes. Some of the cleavage faces suggest that the sandstone is micaceous; this type of sandstone was often split to provide roofing material.

**Category D**

**SITE 3 Drainage system** Located at: SO 0900 3292

A series of water management features on the straight section of stream located to the south of Felinfach, which flows north-eastward into the Afon Dulas. The total length containing recognisable features is approximately 140m, and these include a number of stone built weirs, a stone lined field drain, and occasional stone walled banks perhaps associated with a pitched stone stream bed. Cartographic evidence suggests that the stream was diverted to its present line from the opposite (north-western) side of the A470 between 1814 and 1886; the Talach-ddu Tithe map of 1846 is inconclusive in this respect.

**Category C**

## **7 Recommended Mitigation**

7.1 Possible mitigation strategies for the sites referred to in Section 6, and sites no longer visible on the ground, are considered below.

### **PRN 543 (SAM B49) Moated Site**

This site is located outside the area of the proposed bypass. It is referred to here only in order that inadvertent damage due to ancillary works or passage of machinery can be prevented.

### **PRN 2698 Griffin Inn**

The proposed line of the bypass is adjacent to the rear of the land adjoining the inn, and it seems most unlikely that any direct disturbance to its structure will occur as a result of construction work. It would be preferable for the adjacent part of the bypass to be sympathetically landscaped, in order that the context of the building in its surroundings is not lost.

### **PRN 6736 Felin Cwm Mill**

The location of this site, which is now ruinous, is outside the area affected by the construction of the proposed bypass. In addition, no evidence for any associated water systems (leats) has been found in the area of the proposed bypass; it seems likely that the stream in its entirety was diverted to drive the mill wheel. Consequently, no mitigation is needed in the case of this site.

### **PRN 6790 Felin Cwm Hollow-way**

Examination of the site plans suggests that the western end of the hollow-way will be affected by the construction works; a measured survey of the affected part should be carried out before site work commences.

### **PRN 23022 Felinfach Mill**

The mill is located to the north-west of the A470, and, as such, is outside the area of the proposed bypass. As far as can be ascertained, the water system associated with the mill does not extend to the opposite (south-west) side of the A470, consequently no mitigation is needed in the case of this site.

### **RR 63b Roman road from Brecon to Kenchester**

The line of this former road is crossed by the proposed bypass. Mitigation measures adopted should include: a preliminary survey of extant remains in the two fields centred at SO 0880 3255, archaeological excavation of a section across the road where it is crossed by the line of the proposed bypass (c.SO 0885 3256) to gain more knowledge about its nature and dating, and fencing off sections of the road outside the excavated area to prevent inadvertent damage to surviving remains. Sections of the road which follow the line of the present A470 should be subject to a watching brief during construction.

### **SITE 1 Lower Penishawain House**

The house should not be directly affected by the construction work, although part of the garden is included in the area of the bypass. It would be preferable for the setting of the house not to be significantly altered by the construction of the bypass.

### **SITE 2 Quarry**

It seems likely that only a small length of the quarry will be affected by construction work, and in view of this, no mitigation beyond the watching brief during topsoil stripping, referred to below, is considered necessary.

### **SITE 3 Drainage system**

The apparently integrated system of drainage features, although probably of 19th century date, is fairly unusual and merits further recording. This should take the form of a measured survey of features still extant, prior to the commencement of construction works. In addition, the area should be subjected to a watching brief during topsoil stripping and any associated features revealed similarly recorded.

### **Sites for which no surface traces exist**

It is possible that sites, for which there are no surface traces, still remain to be discovered within the area affected by the proposed bypass; if this is the case, then it seems more likely that these would be prehistoric in date (see para 5.2). A watching brief during topsoil stripping should confirm or deny the presence of sites of this type in the proposal area.

## 8 Acknowledgements

8.1 CPAT would like to thank the landowners and occupiers for allowing access to the route of the proposed bypass.

8.2 In addition, the writer would like to thank the staff of the following organisations for their help and assistance:

The Sites and Monuments Record, Clwyd-Powys Archaeological Trust,  
Welshpool

The National Library of Wales, Aberystwyth

The National Monument Record, RCAHMW, Aberystwyth

The Powys Archive Office, Llandrindod Wells

## 9 Sources

### 9.1 References

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- Jones T., 1952, *Brut y Tywysogion or The Chronicle of the Princes (Peniarth MS.20 Version)*, UWP, Cardiff.
- Margary H., 1992, *The Old Series Ordnance Survey Maps of England and Wales - VI (Wales)*, Lympe, Kent.
- Martin C.H.R. and Walters M.J., 1993, *Brecknock Borough Historic Settlements Survey*, CPAT, Welshpool.
- Pringle J., & George T.N., 1948, *British Regional Geology - South Wales*.
- Rudeforth C.C., Hartnup R., Lea J.W., Thompson T.R.E., and Wright P.S., 1984, *Soils and Their Use in Wales*, Soil Survey of England and Wales Bulletin No 11, Harpenden.
- Smith P., 1988, *Houses of the Welsh Countryside*, HMSO.

### 9.2 Documentary Sources

- 1754 - Jones (D.T.M.) 3, No 316 (National Library of Wales)
- 1765 Perambulation of Talach-ddu Lordship and parish - Maybery 4486 (National Library of Wales)

### 9.3 Cartographic Sources

- 1781 - Tredegar Map Volume 2, Map 4 (National Library of Wales)
- 1814 - Ordnance Surveyors Drawing No 195 (1:31,680)
- 1841 Tithe map of Llanddew, Breconshire
- 1841 Tithe map of Llanfilo, Breconshire
- 1843 Tithe map of Llandefalle, Breconshire
- 1846 Tithe map of Talach-ddu, Breconshire
- 1887 Ordnance Survey First Edition 1:2,500 map, Brecknock 28.3, surveyed 1886
- 1904 Ordnance Survey Second Edition 1:2,500 map, Brecknock 28.2, revised 1903

### 9.4 Aerial Photographic Sources

- RAF/106G/UK 1471 No 3451 (taken 14/5/1946 at a scale of 1:10,000)
- F22/58/RAF/3609 Nos 131-133 (taken 20/6/1960 at a scale of 1:10,000)
- Ordnance Survey 72 328 95-97 (taken 22/8/1972 at a scale of 1:7,600)

## FELINFACH BYPASS: A470

### SPECIFICATION FOR AN ARCHAEOLOGICAL FIELD EVALUATION BY CLWYD-POWYS ARCHAEOLOGICAL TRUST

#### 1 Introduction

1.1 The proposed works affect a corridor of land 7.3m wide by 1.9km long running round the SE side of the village of Felin-fach.

1.2 There are no known archaeological sites within this area though the medieval mill of Cwm Felin and associated trackway lie close by.

1.3 Welsh Office Highways Directorate in consultation with Cadw: Welsh Historic Monuments have determined that a field evaluation and watching brief should form part of this project. Accordingly a brief (dated February 1995) has been prepared by Welsh Office Highways Directorate which describes the scheme of archaeological works required.

#### 2 Objectives

2.1 The objectives of the evaluation are:

2.1.1 to reveal by means of a combination of desk-based assessment and site visits, the nature, condition, significance and, where possible, the chronology of any hitherto undiscovered archaeology within the area of the proposed development in so far as these aims are possible;

2.1.2 to undertake documentary research on the mill at Cwm Felin;

2.1.3 to locate hitherto unknown archaeological features by means of a watching brief during the topsoil stripping in preparation for the road construction;

2.1.4 to record, excavate and preserve by record any archaeological deposits revealed during the watching brief in the road corridor;

2.1.5 to prepare a report outlining the results of the fieldwork.

#### 3 Methods

3.1 Stage one of the evaluation will involve the examination of all the readily available primary and secondary records relating to the mill and affected area, including documentary and cartographic sources. Archives and repositories will include the County Sites and Monuments Record, the County Record Office and the National Library of Wales.

3.2 Stage two will take the form of a site visit and survey along the road corridor.

3.3 The watching brief will coincide with the mechanical topsoil stripping by Welsh Office Highways' contractors of the road corridor.

3.3.1 all archaeological contexts recorded using the standard numbered context system employed by CPAT. All significant contexts to be planned and/or

drawn in section at appropriate scales (as defined in the Project Brief), and photographed in monochrome and colour. All drawn records will be related to control points depicted on modern maps.

3.3.2 all archaeological artefacts and environmental samples recorded and processed in a manner appropriate to the material involved. Those requiring conservation or other specialist treatment will be stored in a stable environment until such times as they can be examined by a specialist. All finds, except those deemed to be Treasure Trove, are the property of the landowner. It is anticipated that they will be donated to the appropriate local or regional museum, subject to agreement being reached with the landowner and the museum curator.

3.4 Following the on-site work an illustrated and bound report will be prepared according to the principles laid out in the Project Brief (section 4). This will be in A4 format and contain conventional sections on: Site location, Topography and Geology; Historic Background; Excavation; Conclusions and References, together with appropriate appendices on archives and finds.

3.5 The site archive will be prepared to specifications laid out in Appendix 3 in the Management of Archaeological Projects (English Heritage, 1991).

#### **4 Resources and Programming**

4.1 The fieldwork will be undertaken by a small team of 1 or 2 skilled archaeologists under the direct supervision of an experienced field archaeologist, who will also be responsible for undertaking the desk-based assessment. Overall supervision will be by Dr A Gibson, a senior member of CPAT's staff who is also a member of the Institute of Field Archaeologists.

4.2 All report preparation will be completed by the same field archaeologist who conducted the fieldwork.

4.3 It is anticipated that the fieldwork will take no more than four days in all and that the subsequent report would be prepared immediately after the watching brief dependent on the client's instructions and the arrangement of a suitable timetable. The date of commencement, at the time of writing, has yet to be agreed with the client, and will be dependent on the client's programme of works. The archaeological curator will be informed of the detailed timetable and staffing levels when agreement has been reached with the client.

4.4 Requirements relating to Health and Safety regulations will be adhered to by CPAT and its staff.

4.5 CPAT is covered by appropriate Public and Employer's Liability insurance.

A.M. Gibson  
2nd March 1995

## APPENDIX 2

### EXCAVATION OF ROMAN ROAD (RR 63b) (Figs 3 and 4)

#### 1 Introduction

- 1.1 The probable line of Roman road 63b was identified in the initial appraisal of the bypass route, to which this appendix is added. In light of the fact that the former road line would be cut by the bypass, the Welsh Office decided that excavation and recording of a section of the road, in line with the mitigation strategy suggested in this report, should be carried out.
- 1.2 The excavation took the form of a single trench, Trench A (c. 5 x 10), excavated by machine. The trench was then cleaned by hand and any features seen were investigated and recorded.

#### 2 Excavation (numbers in brackets refer to Fig. 3)

- 2.1 Only one feature was apparent within the trench, which proved to be a linear hollow or ditch (5), approximately 1.0m wide and 0.2m deep, which crossed the trench at right-angles on an alignment of 357 degrees (magnetic). The ditch was lined with small stones set in the natural red clay subsoil, to form a hard compacted surface (6); it was filled with smooth red clay (4) which contained some small stones. No artifacts were recovered.
- 2.2 In view of the compacted stone surface or metalling (6) found in the ditch (paragraph 1.3), a road surface composed of similar metalling would have been expected adjacent to the ditch. Three possible areas of metalling (of maximum width 0.8m) were recognised in the trench sections, although these occurred on both sides of the ditch.

#### 3 Conclusions

- 3.1 The ditch (5) revealed by the excavation provides some additional support to the suggestion of a Roman road at this location. However, no definite metalled surface was identified adjacent to the ditch. Also, it would be usual for a road to be lined by drainage ditches on either side, and the possible metalling identified in section lay on both sides of the ditch, so that the precise position of the road remains unclear; the topography suggests that it would be to the east of the ditch, and this may be confirmed by the proposed watching brief, to be carried out during topsoil stripping.
- 3.2 From the surviving evidence, it is likely that the road would have been metalled with small stones set in the natural clay subsoil; this putative surface appears to have been lost as a result of ploughing after 1946, when the RAF aerial photograph, mentioned in Section 6 of this report, was taken.
- 3.3 The adjacent field to the south, through which the road runs, may well contain better preserved remains of the road as it does not appear to have been ploughed on as regular a basis as the field in which the excavation was carried out. However, the better preserved areas are located outside the road corridor, and were not therefore available for excavation.

## APPENDIX 3

### SURVEY OF DRAINAGE SYSTEM AND ROMAN ROAD EARTHWORKS

#### 1 Introduction

- 1.1 A series of water management features were identified during the original Fieldwalk Survey (SITE 3), and were subsequently recommended for a detailed measured survey. Further survey was also recommended for the line of the Roman Road RR 63b where extant earthworks survived within the road corridor.

#### 2 Roman Road: Earthwork Survey (Fig. 4)

- 2.1 Earthworks possibly relating to a Roman Road survived in two adjacent fields to the south of Felinfach, both of which lay within the proposed road corridor. A detailed EDM survey was conducted of both sets of earthworks, the results of which are shown in Fig. 4).
- 2.2 In the southern field, two low banks up to 0.6m high define the sides of a trackway curving slightly, but aligned generally NNE-SSW, immediately to the west of the field boundary. The earthworks extend for c. 175m, and suggest an average width of 10m for the road.
- 2.3 In the northern field, the road survives as a slight hollow way c. 9.5m wide, running roughly N-S for c. 90m. The archaeological evaluation, Trench A, was located towards the southern end of the feature.

#### 3 Drainage System: Measured Survey (Fig. 5)

- 3.1 A series of three weirs were identified on a tributary of the Afon Dulas, and these were recorded in detail by an EDM survey, the results of which are depicted in Fig. 5.
- 3.2 The upper weir, Weir 1, was perhaps the best preserved of the three, and consisted of four surviving stone slabs (average c. 0.7 x 1.7m) visible above the water level. These extended from the SE bank of the stream for 3.5m. On the NW side of the stream the remains of a stone culvert were recorded, consisting of two dry-stone walls with small slabs across the top. The culvert was aligned roughly E-W and extended into the bank at the western end.
- 3.3 Weir 2 was located 40m downstream of Weir 1. A stone wall lined the SE bank of the stream, with the weir extending for 2m towards the middle of the stream. The construction was similar to Weir 1, with three slabs surviving, but with no sign of a culvert along the NW bank.
- 3.4 Weir 3 was the least well preserved, with little surviving stone remaining in situ. The weir lay 43m downstream of Weir 2, 150m from the confluence with the Afon Dulas.
- 3.5 The function of the weir system remains uncertain, although it may relate to flood control. It would seem likely that the weirs are of 19th-century date, since there is cartographic evidence

to suggest that the stream was diverted to its present course between 1814 (Ordnance Surveyor's Drawing No 195) and 1886 (1st Edition Ordnance Survey).

## APPENDIX 4

### A470 FELINFACH BYPASS: ARCHAEOLOGICAL ASSESSMENT

#### SPECIFICATION FOR AN ARCHAEOLOGICAL ASSESSMENT TO BE CONDUCTED BY CLWYD-POWYS ARCHAEOLOGICAL TRUST

## **1 Introduction**

1.1 The proposed works affect a corridor of land 7.3m wide by 1.9km long running round the SE side of the village of Felin-fach.

1.2 Two archaeological sites identified during the initial archaeological survey have been deemed to be worthy of further archaeological work: namely the Brecon-Kenchester Roman Road (RR63b) and an area of drainage works (CPAT Report 141, site 3).

1.3 Welsh Office Highways Directorate in consultation with Cadw: Welsh Historic Monuments have determined that these sites should be recorded prior to their destruction. Accordingly a brief (dated 16th June 1995) has been prepared by Welsh Office Highways Directorate which describes the scheme of archaeological works required.

## **2 Objectives**

2.1 The objectives of the evaluation are:

2.1.1 to record by survey the drainage system (site 3) at a scale of at least 1:500, with detailed insets as necessary, and to prepare a plan linked to existing topographical features;

2.1.2 to further record by means of a watching brief any additional features of site 3 during the topsoil stripping of the site;

2.1.3 to record by survey and trial excavation, the Brecon to Kenchester Roman Road (RR63b) where it crosses the site, to prepare a plan of the earthwork features at 1:500 or larger and to record two sections through the feature.

2.1.4 to prepare a report outlining the results of the fieldwork.

## **3 Methods**

3.1 The survey of both areas of archaeological interest and the excavation of the Roman road will take place prior to the commencement of topsoil stripping in the specific areas.

3.2 The watching brief of site 3 will coincide with the mechanical topsoil stripping by Welsh Office Highways' contractors of the road corridor.

3.2.1 all archaeological contexts recorded using the standard numbered context system employed by CPAT. All significant contexts to be planned and/or drawn in section at appropriate scales (as defined in the Project Brief), and photographed in monochrome and colour. All drawn records will be related to control points depicted on modern maps.

3.2.2 all archaeological artefacts and environmental samples recorded and processed in a manner appropriate to the material involved. Those requiring conservation or other specialist treatment will be stored in a stable environment until such times as they can be examined by a specialist. All finds, except those deemed to be Treasure Trove, are the property of the landowner. It is anticipated that they will be donated to the appropriate local or regional museum, subject to agreement being reached with the landowner and the museum curator.

3.3 Following the on-site work an illustrated and bound report will be prepared according to the principles laid out in the Project Brief. This will be in A4 format and contain conventional sections on: Site location, Topography and Geology; Historic Background; Survey; Excavation; Conclusions and References, together with appropriate appendices on archives and finds.

3.4 The site archive will be prepared to specifications laid out in Appendix 3 in the Management of Archaeological Projects (English Heritage, 1991).

#### **4 Resources and Programming**

4.1 The fieldwork will be undertaken by a small team of 1 or 2 skilled archaeologists under the direct supervision of an experienced field archaeologist, who will also be responsible for undertaking the desk-based assessment. Overall supervision will be by Dr A Gibson, a senior member of CPAT's staff who is also a member of the Institute of Field Archaeologists.

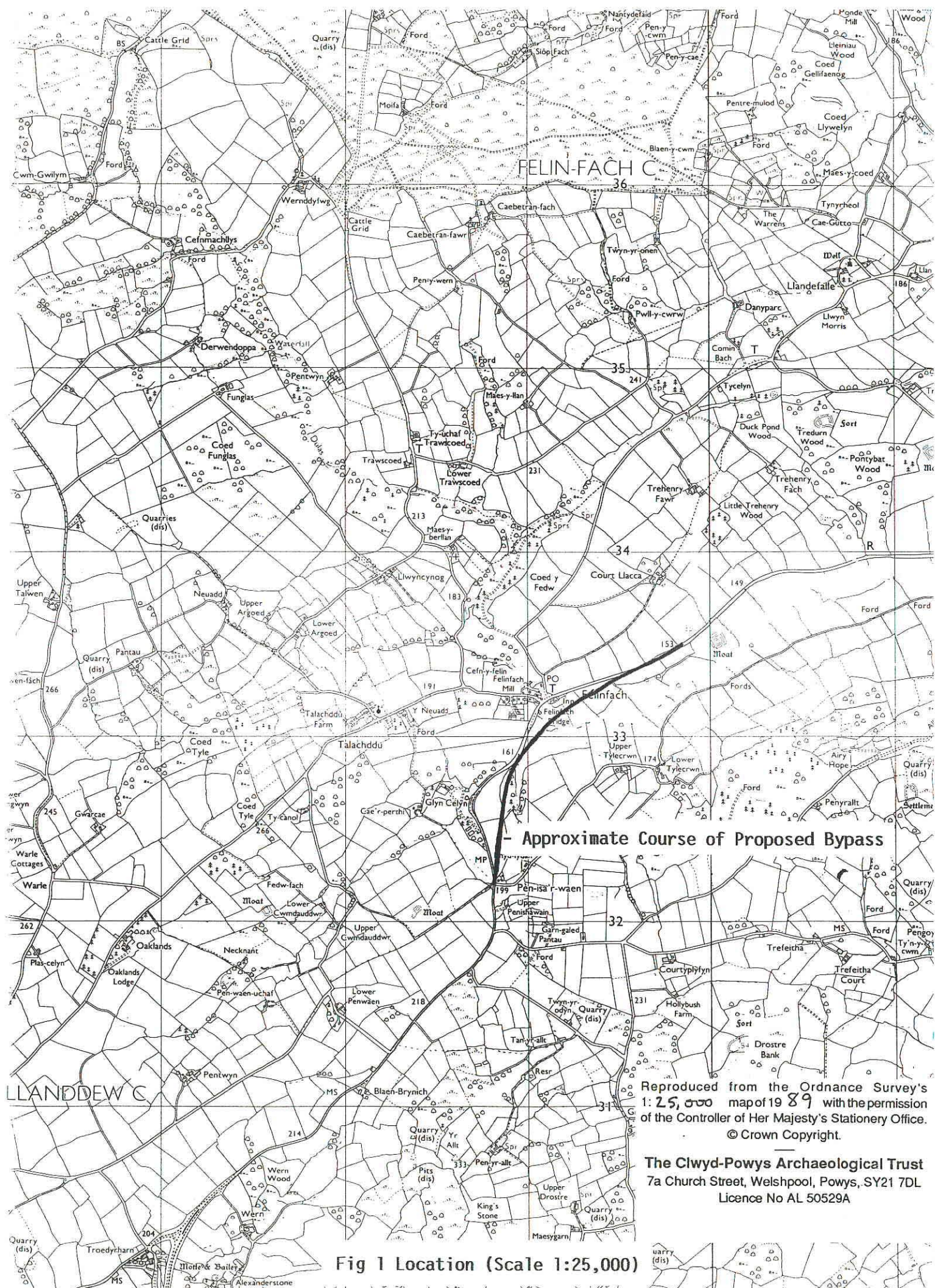
4.2 All report preparation will be completed by the same field archaeologist who conducted the fieldwork.

4.3 It is anticipated that the fieldwork will take no more than five days in all and that the subsequent report would be prepared immediately after the watching brief dependent on the client's instructions and the arrangement of a suitable timetable. The date of commencement, at the time of writing, has yet to be agreed with the client, and will be dependent on the client's programme of works. The archaeological curator will be informed of the detailed timetable and staffing levels when agreement has been reached with the client.

4.4 Requirements relating to Health and Safety regulations will be adhered to by CPAT and its staff.

4.5 CPAT is covered by appropriate Public and Employer's Liability insurance.

A.M. Gibson  
20th June 1995



- Approximate Course of Proposed Bypass

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Fig 1 Location (Scale 1:25,000)

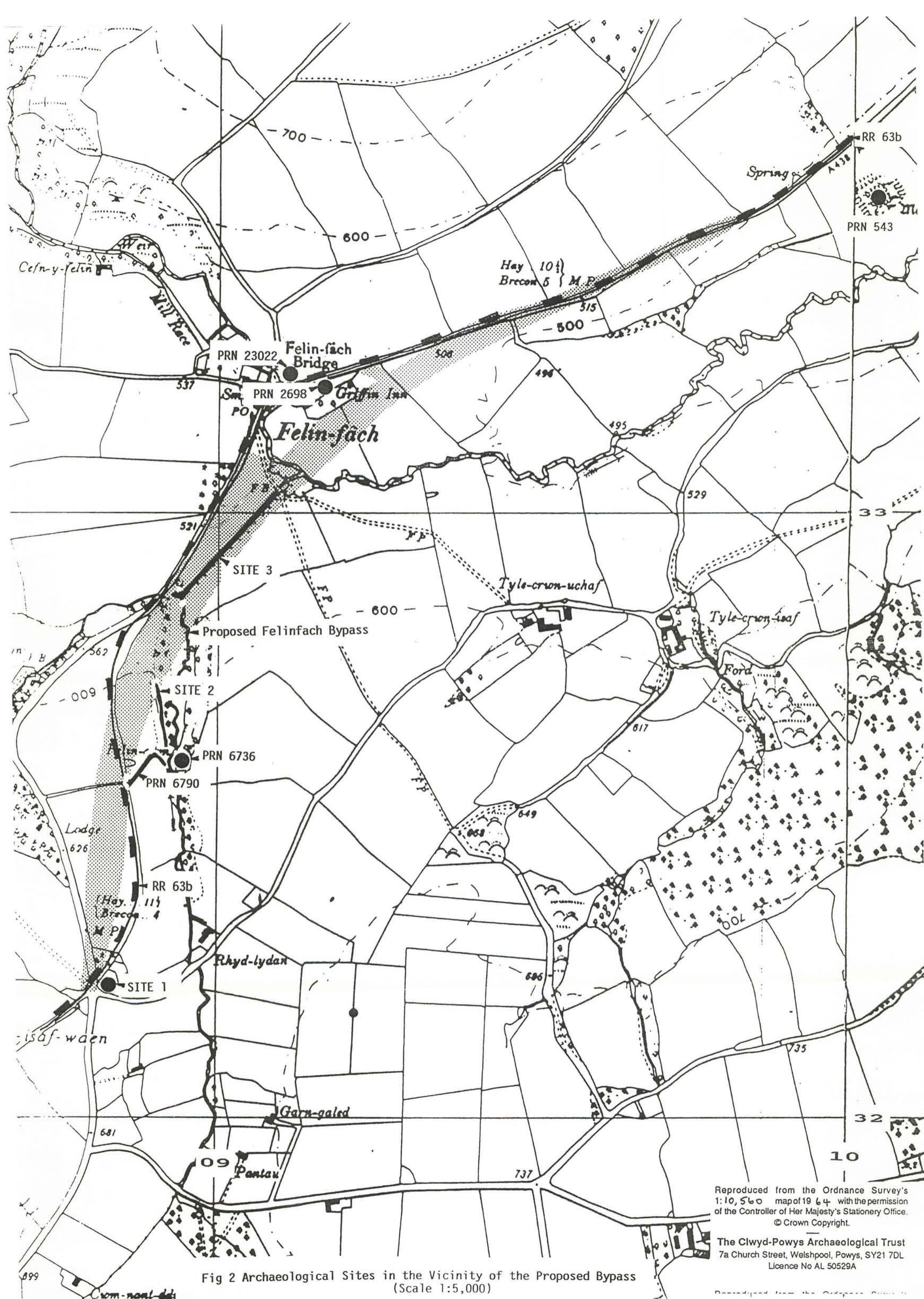


Fig 2 Archaeological Sites in the Vicinity of the Proposed Bypass  
(Scale 1:5,000)

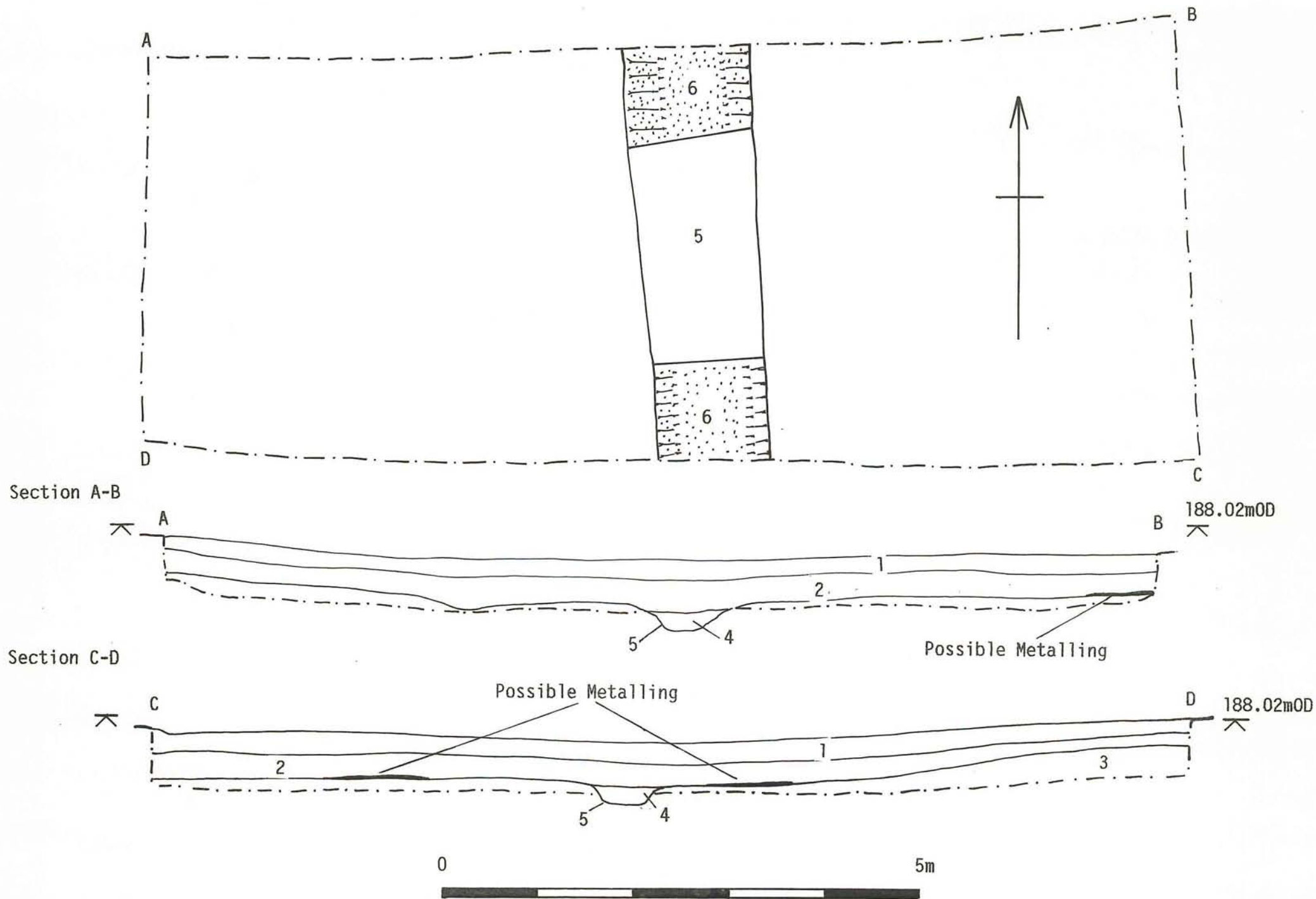


Fig. 3. Roman Road Excavation. Plan and Sections (Scale 1:50)

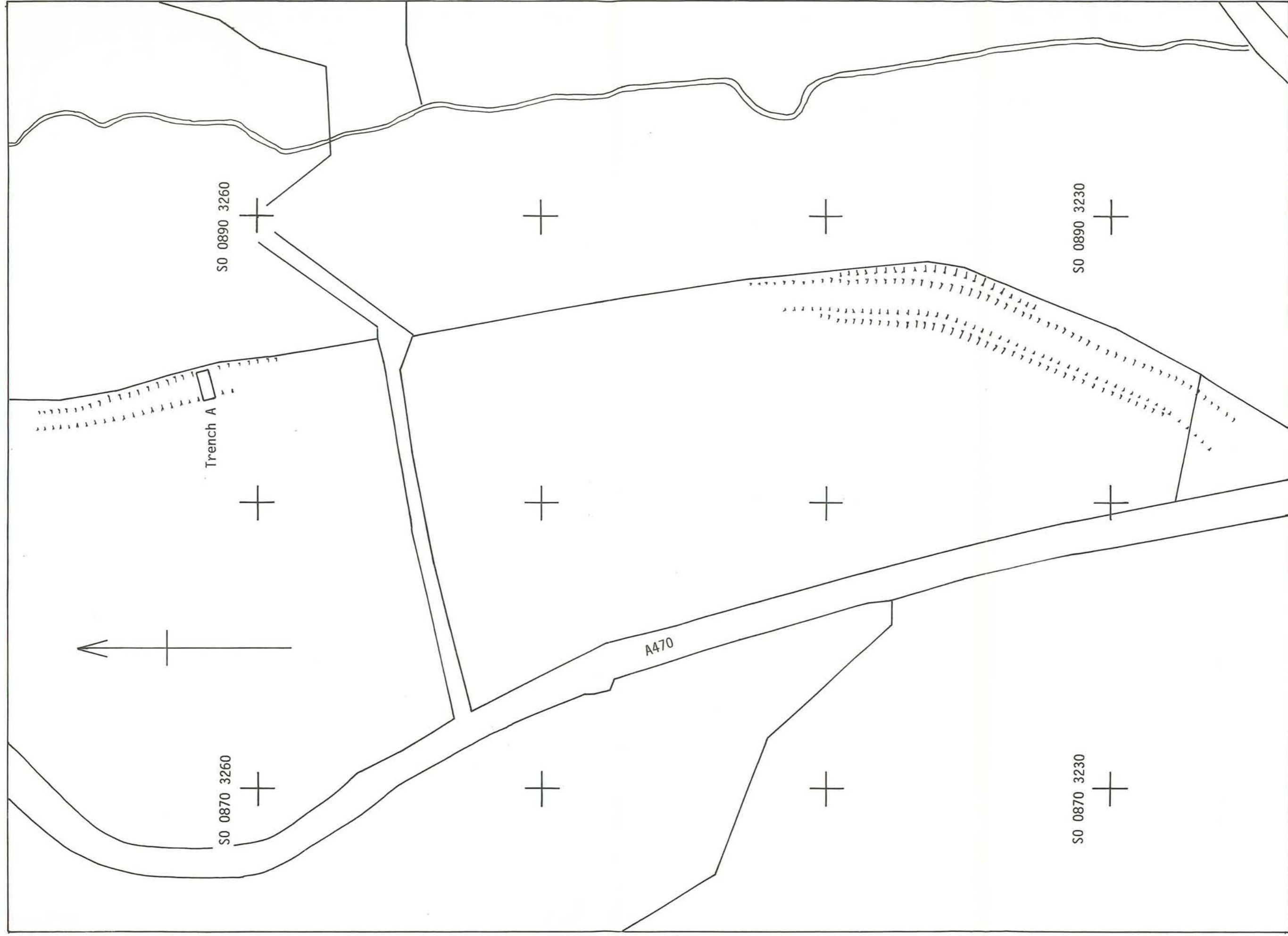


Fig. 4. Roman Road Earthwork Survey and Trench location. Scale 1:1250

# FELINFACH

FIG 5 IS A CAD DRAWING  
ON DISK - FELWEIR.PLT

TO PRINT, USE A3 PLOTTER,

INSERT DISK AND TYPE:-

PRINT A:FELWEIR.PLT (RTN)

Attrs - Name & list device (Prt): (RTN)

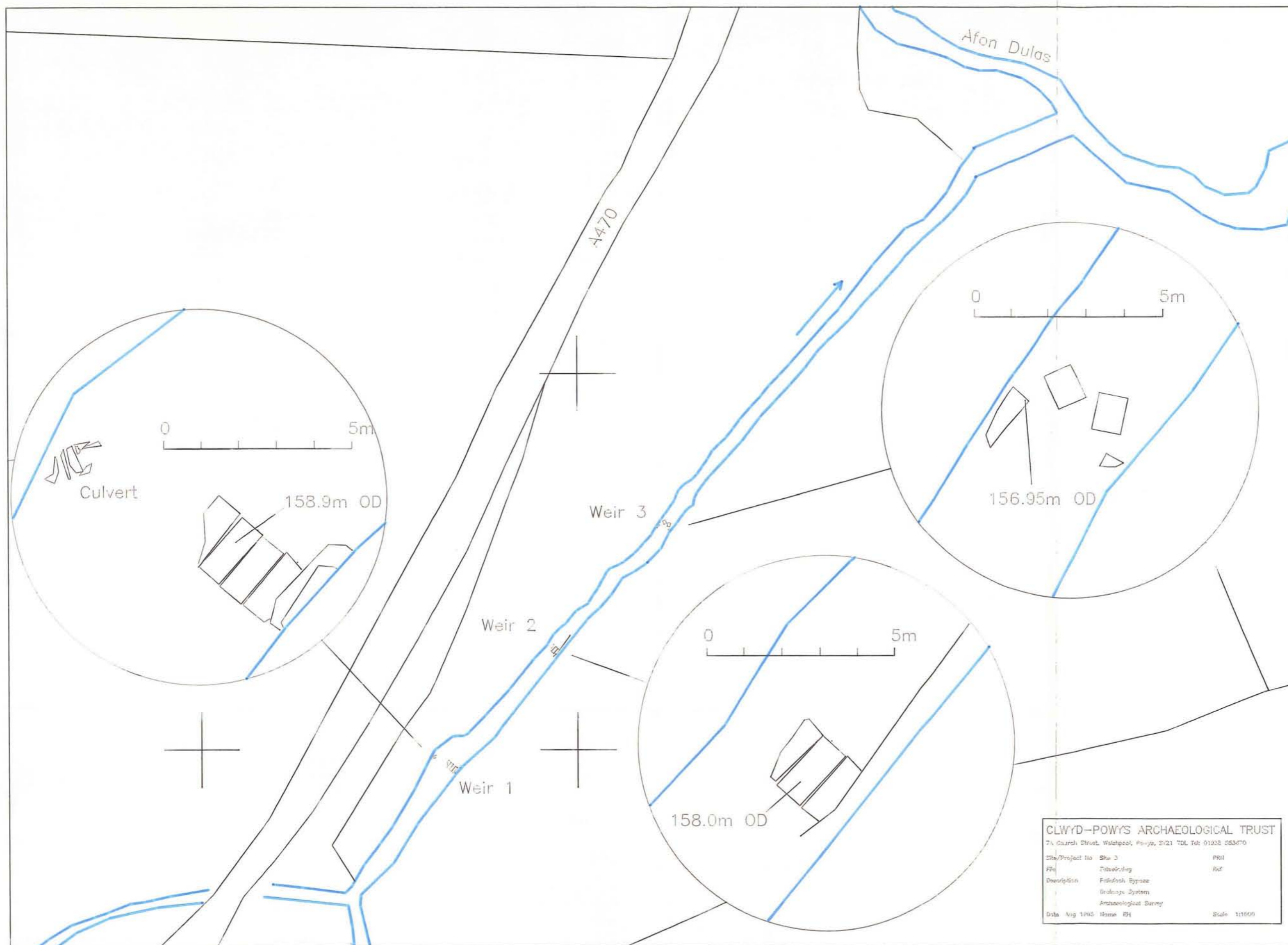


Fig 5: Drainage System – Archaeological Survey. Scale 1:1000, insets at 1:100