

THE CLWYD-POWYS ARCHAEOLOGICAL TRUST

Former Travis Perkins Site, Welshpool

ARCHAEOLOGICAL ASSESSMENT



CPAT Report No 893

Former Travis Perkins Site, Welshpool

ARCHAEOLOGICAL ASSESSMENT

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October 2007

Report for Hughes Architects Ltd

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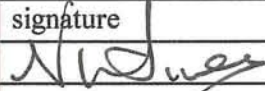
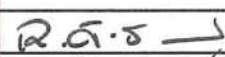
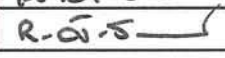
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1 INTRODUCTION

- 1.1 In September 2007, the Field Services Section of the Clwyd-Powys Archaeological Trust was invited by Hughes Architects to undertake an archaeological assessment in connection with proposals for a new development on land at the former Travis Perkins builders' merchants yard, and around Dolanog Cottages, off Severn Street, in Welshpool. The Curatorial Section of the Clwyd-Powys Archaeological Trust, in their capacity as archaeological advisors to the local authority, had determined that an archaeological assessment was required to identify the potential impact on the archaeological resource. Accordingly, a brief was prepared which detailed the works required (CPAT EVB 670).
- 1.2 The assessment was required as information retained within the Historic Environment Record indicated that during the later 19th century the site had been occupied by a canal maintenance yard which included a narrow gauge railway and a number of buildings, some of which are still standing.

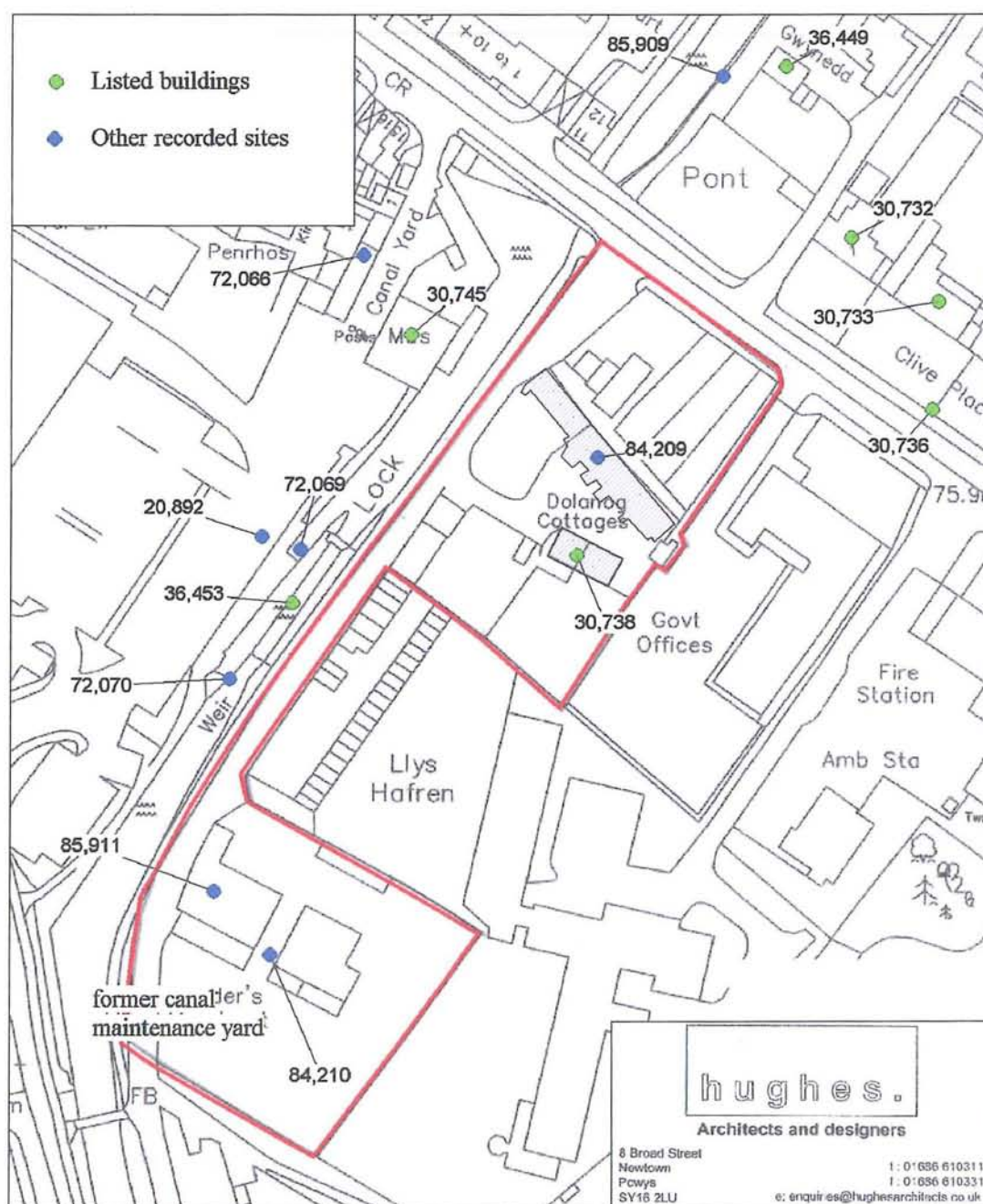


Fig. 1 Area of the proposed development (outlined in red) and surrounding archaeological sites and listed buildings recorded in the Historic Environment Record

2 LOCATION, TOPOGRAPHY AND GEOLOGY

- 2.1 The proposed development is located on the east side of the Montgomeryshire Canal and south of Severn Street in Welshpool, centred at SJ 22580722 (Fig. 1).
- 2.2 The solid geology of the area mainly consists of undivided Ludlow and Wenlock Series siltstones and mudstones belonging to the Silurian period, though there is some local faulting which has exposed Caradoc Series siltstones and mudstones of the Ordovician period (1994 British Geological Survey map). The soils of the area generally consist of fine silty and loamy soils belonging to the Denbigh 1 Association (1983 Soil Survey of England and Wales map).

3 DESK-BASED STUDY

- 3.1 The desk-based study involved the examination of all the readily available primary and secondary documentary, cartographic, pictorial, and photographic sources for the immediate area. Repositories consulted included the following: the Regional Historic Environment Record (HER), held by CPAT at Welshpool; the National Monuments Record, maintained by the Royal Commission on Ancient and Historical Monuments in Wales (RCAHMW) in Aberystwyth; the National Library of Wales in Aberystwyth; and Powys County Archives in Llandrindod Wells.

Prehistoric period

- 3.2 Although there are no recorded prehistoric archaeological sites in the vicinity of the proposed development, there is nevertheless the potential for buried deposits of this period within the area. Evidence from the wider area around Welshpool suggests that this part of the Severn Valley was extensively occupied during prehistory, particularly from the Neolithic onwards. To the south of Welshpool there is a nationally important complex of Neolithic and Bronze Age funerary and ritual monuments, while to the north of the town there is evidence for later prehistoric farming in the form of defended enclosures and field systems. In general, sites of this period are located on the river terrace above the floodplain, in a similar position to that of the proposed development.

Romano-British period

- 3.3 Although the origins of the present town lie in the medieval period, there is evidence of occupation dating back to the Roman period. During construction works at the Smithfield in 1959 a probable burial deposit of Roman date was found, and this was partially excavated by the National Museum of Wales some months later, in July 1960. All of the objects recovered were consistent with grave goods, although no trace of a pit, cist or other burial feature was identified.
- 3.4 The artefacts were of extremely high quality and included three bronze paterae (dishes) and a bronze ewer (jug), which were placed in a bronze cauldron and wrapped in a fine linen cloth and may have been packed in leaves. A sixth vessel was a wooden bucket with a bronze bull's head handle, similar to an example from Kent. There was also an iron fire-dog which dates from AD 150-200, two iron stands, the remains of a glass bottle and some sherds of pottery. From the nature of the artefacts George Boon of the National Museum concluded that they were likely to be associated with the grave of a native Cornovian chieftain.

Early medieval period

- 3.5 Welshpool is said to have been the site of churches founded by St Cynfelyn and his brother Llywelyn in the 6th century AD; the foundation of the latter has since been associated, at least traditionally, with the 'Old Church' which is known formerly to have stood on the east side of Mill Lane, immediately to the south of the present Salop Road. The most recent church on the site was erected in 1587 but was destroyed by fire in 1659, though some of its masonry remains were still visible until the 19th century (Soulsby 1983, 265).

Medieval period

- 3.6 Domen Gastell, a motte and bailey castle lies 300m to the east-north-east of the proposed development area. It may have been constructed as early as 1111, although the earliest possible reference to it dates from 1196 (Silvester 1992, 167). There has been some suggestion that a settlement developed around the castle, but no firm evidence has yet been found to substantiate this hypothesis.
- 3.7 The present town of Welshpool resulted from a deliberate attempt by Gruffudd ap Gwenwynwyn, Prince of Powys, to establish a new borough. As such, this was a planned town which owed little to any existing settlement (Soulsby 1983, 266). The burgesses received a foundation charter in the 1240s and a market was recorded there in 1252 (Silvester 1992, 167). The new town adopted a basically linear plan, centred on High Street and Broad Street, with some development also along Church Street, Berriew Street and Severn Street.

Post-medieval and modern

- 3.8 The earliest map of Welshpool is by Humphrey Bleaze, dated 1629 (Fig. 2), which unfortunately does not show the area of the proposed development, although it does depict a number of buildings along the southern side of Severn Street.

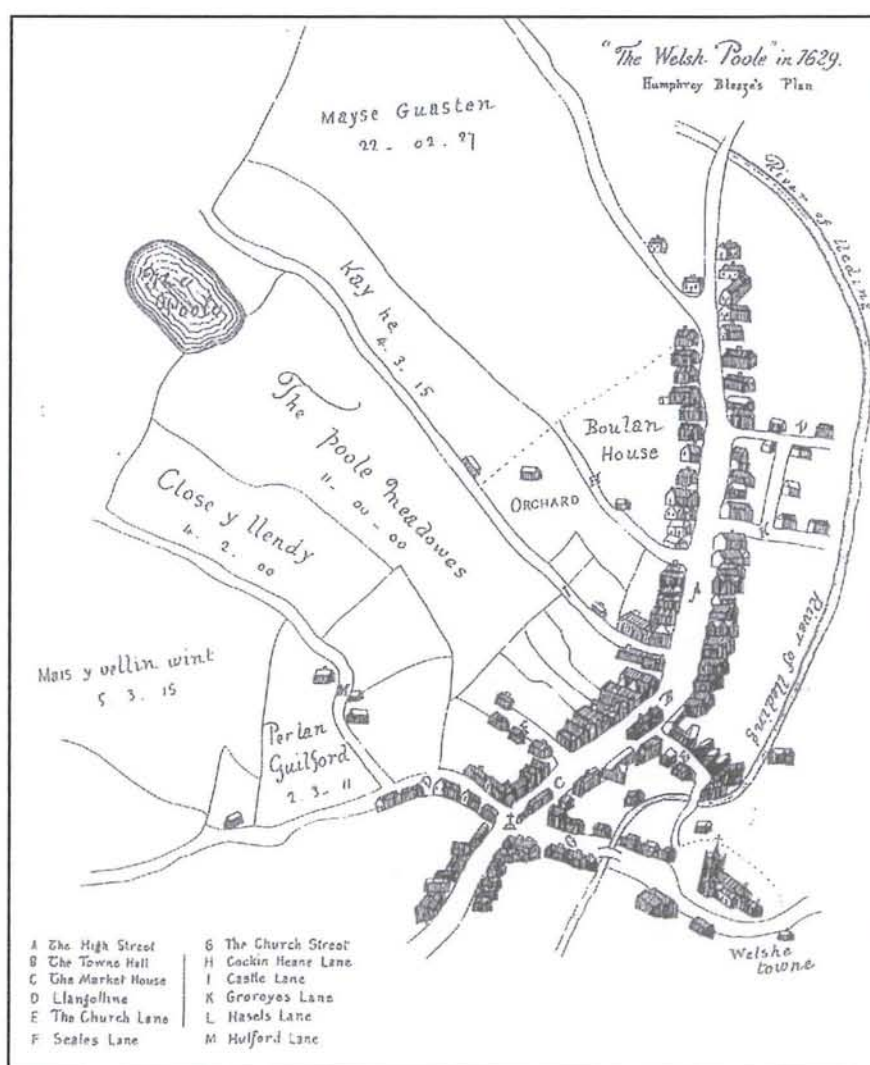


Fig. 2 Map of Welshpool by Humphrey Bleaze, 1629

- 3.9 A survey of the Estate of Mrs Victoria Lloyd, surveyed by John Rocque in 1747 shows a watercourse running roughly west to east across the area, to the south of which, in the area later occupied by Travis Perkins, the map depicts what appear to be strip fields. A group of buildings is depicted further to the north, which are identified on later maps as Sergeant's Row, Dolanog House, and Dolanog Cottages. The condition of the map and copyright restrictions prevent its reproduction in this report.
- 3.10 The first accurate cartographic depiction of Welshpool as a whole is provided by a post-1780 map of Powis Estate (NLW/Powis Castle/M17-18), and a later version of the same map dated 1800 (NLW/Powis Castle/M20). Copyright restrictions prevent the reproduction of either map in this report. The earlier map depicts the town before the construction of the Montgomeryshire Canal, which appears on the map of 1800. Interestingly, both maps depict a different line for the eastern end of Severn Street than its present course. From a point just west of the canal bridge the road formerly deviated slightly to the south, passing immediately along the northern side of properties identified on later maps as Sergeant's Row and Dolanog House, to the rear of which a building is marked in the position of Dolanog Cottages. The part of the development area formerly occupied by Travis Perkins is not shown in detail, but is named as 'Upper Maes y Dre'. On the opposite side of the canal the 1800 map depicts a wharf and canal basin immediately to the south of the canal bridge carrying Severn Street. Comparison with later maps clearly indicates that this basin is not the one that survives today which occupies the site of the former road bridge.
- 3.11 The waterway now known as the Montgomery Canal was built in stages between 1794 and 1821, and ran from the Shropshire Union Canal at Frankton Locks near Oswestry to Newtown, reaching Welshpool by 1797. The canal was an agricultural rather than an industrial waterway and was primarily constructed to carry and distribute lime for agricultural purposes from the Llanymynech quarries (Hughes 1988, 9).
- 3.12 Welshpool was the only town on the original line of the canal and became the administrative centre of the waterway, with important wharfages developing at Severn Street and the adjacent Hollybush Wharf, and on the north side of Severn Street as far as the Lledan Brook Aqueduct (Hughes 1988, 120-4).

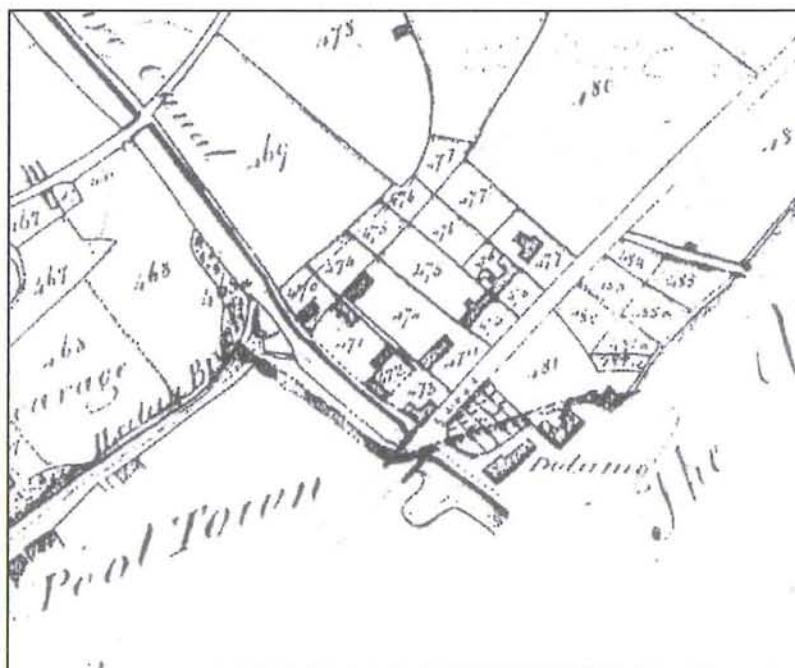


Fig. 3 1840 Tithe Survey for Pool parish, Lower Division

- 3.13 By the time of the 1840 Tithe Surveys for Pool parish Upper and Lower Divisions (Figs 3-4) the line of Severn Street had been changed, adopting its present, straight course east of the canal. Two buildings are depicted at the southern end of the proposed development site, which was owned by the Earl of Powis and leased to David Jones as a timber yard.

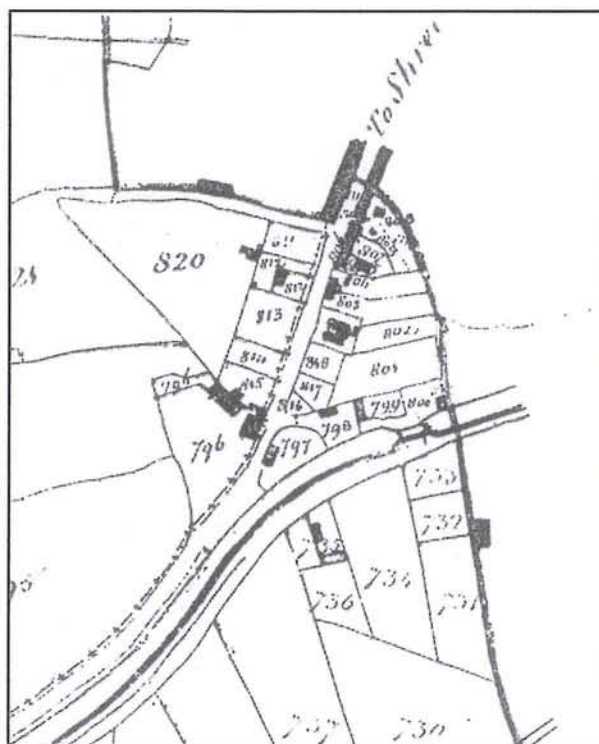


Fig. 4 1840 Tithe Survey for Pool parish, Upper Division

- 3.14 The large-scale 1885 Ordnance Survey map (Fig. 5) shows the development area in some detail. The houses known as Dolanog and Dolanog Cottages are depicted, together with Sergeant's Row, while the area between Sergeant's Row and Severn Street is shown as gardens. At the southern end of the proposed development site the timber yard had been replaced by the buildings of a new canal maintenance yard (PRN 85911). On the opposite side of the canal the map depicts the Town Lock Corn Mill which was sited on the by-pass of the town lock (PRN 36453), taking advantage of the water power from the weir, while not interfering with trade on the canal. The mill is first recorded in the Montgomeryshire Canal Report Books in January 1838 but curiously is not recorded by the 1840 Tithe Survey. The mill (PRN 20892) and its waterwheel (PRN 72069) are visible on an early 20th-century postcard (Plate 2). The map also shows the buildings of Hollybush Wharf, none of which survive, and Canal Wharf, the warehouse for which is now occupied by the Powysland Museum (PRN 30745).
- 3.15 The general area had changed little by the time of the 2nd edition map of 1902 (Fig. 6), although there had been further development at the canal maintenance yard, with the erection of new buildings and the construction of a narrow gauge railway linking the works to the canal, and including a canalside crane.



Fig. 5 Ordnance Survey 1:500 plan of Welshpool, 1885



Fig. 6 Ordnance Survey 2nd edition 1:2,500 map of 1902

- 3.16 A postcard, possibly dating from the late 19th century (Plate 1) depicts Sergeant's Row and clearly shows two phase of building with the three cottages nearest the canal being of timber-frame construction with thatched roofs. The rest of the row appears to be built in brick and the whole row is painted with an elaborate mock timber-frame design.



Plate 1 Postcard view of Sergeant's Row, possibly late 19th-century

- 3.17 An early 20th-century postcard (Plate 2) depicts part of the proposed development site, showing the end of Sergeant's Row and the canal maintenance yard to the left of the towpath, with the warehouse of the Canal Wharf and Town Lock Corn Mill on the right.

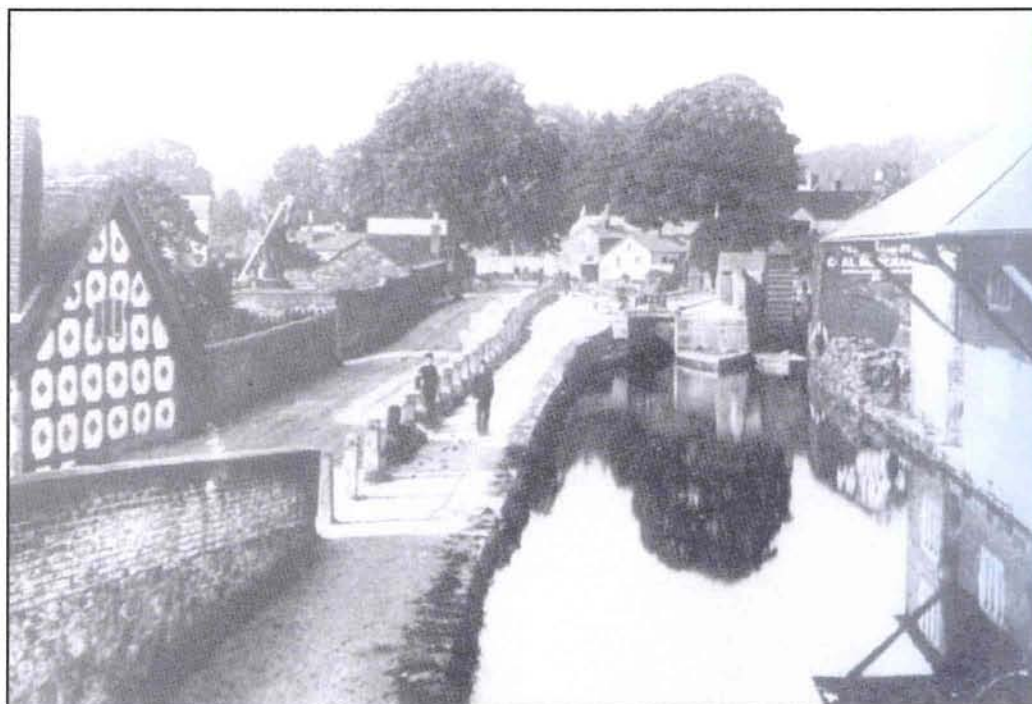


Plate 2 Early 20th-century postcard view of Welshpool Town Lock

4 BUILDING SURVEY

- 4.1 A rapid building survey was conducted on 25 September 2007, comprising a basic descriptive record and high resolution digital photographs of all buildings, including interior and exterior views, together with views of any original fixtures and fittings, though it was subject to access and health and safety considerations. Copies of the digital images have been passed to the client, curator and regional Historic Environment Record.

Dolanog Cottages (PRN 30738)

- 4.2 The property known as Dolanog Cottages lies 50m to the south-west of Severn Street, and 30m south-east of the canal. The building is depicted on a late 18th-century estate map and an inspection of its construction suggests that the earliest phase is 18th-century in date, or possibly slightly earlier. The building is generally in a very poor condition and it was not possible to gain access to the first floor due to health and safety considerations.



Plate 3 Northern elevation of Dolanog Cottages

- 4.3 The original building was a single-storey, timber-framed house, measuring approximately 9.5m by 6.9m. Much of the original framing is now missing or hidden behind later plaster, vegetation and recent shuttering. However, the western gable is exposed (Plate 4), revealing a truss with raking queen struts which has cut-outs for three purlins on the south side, but only two on the north side. The framing below the tie beam has been removed and replaced in brick, with the exception of the corner posts. The visible framing on the south elevation suggests that the present brick infill is not original as there are several peg holes indicating the position of rails no longer present – it is possible that the original infill was wattle and daub, although there is no visible evidence for this. There is an axial stack which may be an extension of an original chimney, and could have formed a central lobby entrance.



Plate 4 Western gable of Dolanog Cottages showing original roof line

- 4.4 At a later date, probably during the 19th century, the roof was raised by the addition of brick to the gable end and timber-framing with brick infill above the original wall plate. The new slate roof included two gabled dormer windows on the south side and one on the north side, with a brick axial stack. Possibly contemporary with these alterations is the addition of a single-storey range on the east end of the building, which is now roofed with corrugated iron but which was presumably originally slate, with a chimney stack to the north side of the apex. The northern wall of the extension is constructed of fairly slight timber-framing with brick infill, while the eastern gable is entirely in brick. The framing at the eastern end of the north wall appears to have been truncated and there is no corner post as one might expect, suggesting that the eastern end of the extension has been removed. Comparison with the 1:500 Ordnance Survey map of 1885 indicates that the extension was originally around 7.4m in length with a lean-to structure on the south-east corner, whereas it is now only 4m long.
- 4.5 The 1885 map shows the building divided into two cottages, suggesting that the eastern extension was originally constructed as a separate dwelling. In its present form, however, the building is partitioned into two cottages with the dividing wall against the stack of the earlier house. The ground floor of the western cottage comprises a living room/kitchen from which stairs rise to the first floor and a door leads to two service rooms at the rear. The first floor is divided into two bedrooms, the southern room having a dormer window and the other a window in the northern gable.
- 4.6 The ground floor of the eastern cottage comprises a living room, from which stairs rise to the first floor and a door leads to a rear door flanked by two service rooms. A second door leads into the eastern room which has a fireplace on the northern wall. The first floor is divided into two bedrooms, north and south, both with dormer windows.

Former Canal Maintenance Yard

- 4.7 The range of buildings formerly occupied by Travis Perkins builders' merchants are the original buildings for the Shropshire Union Railways and Canal Company maintenance yard, dating from the second half of the 19th century. Prior to the construction of the yard, canal maintenance had been carried out from a yard and sheds opposite the Aqueduct Warehouse in Welshpool. The maintenance yard was a substantial complex, whose construction implies substantial investment by the canal's management. The buildings can be identified in an early 20th-century photograph (Plate 2), and the Ordnance Survey maps of 1885 and 1902 (Figs 5 and 6) provide detail of the early development of the yard.

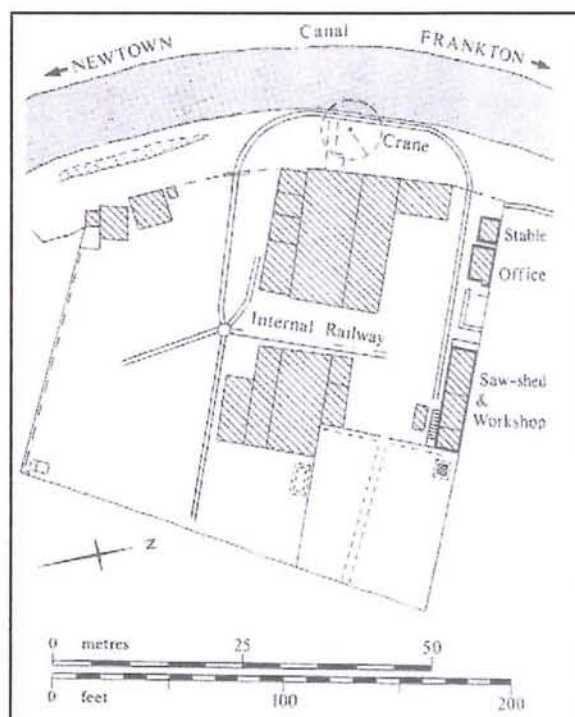


Fig. 7 Plan of the maintenance yard (after Hughes 1988, Fig. 97)

- 4.8 By 1902 the yard was equipped with a waterside crane (now lost), a tramway and a wagon turntable to ease the movement of heavy items about the site. A curious feature of the yard is that there is no evidence of powered machinery; no boiler house or chimney can be identified, no water power was employed, and the date of construction makes it unlikely that a gas engine could have been used, at least at first. It is hard to believe that the canal itself relied on the labour of men in a sawpit, when less than a mile away was the Powis Estate sawmill, with powered machinery that had been driven by canal water since the 1820s. The maintenance yard is a particularly noteworthy survival, in view of its date and its completeness (Eaton 2003). The site ceased to be used for canal purposes from around 1926 and was leased by Building Material Supply Stores Ltd from 1932, subsequently being taken over by BMSS and then Travis Perkins.
- 4.9 The majority of the associated buildings still survive, although a number have been demolished, including the office and stables, which were recorded during the 1980s by the Ironbridge Institute and RCAHMW (Fig. 7). The surviving buildings include the workshops (Plate 5) which are built in red brick with brick floors, forming two adjacent ranges of which the southern is the older, perhaps dating to the mid-19th century, and has timber louvered vents along the apex of the roof. Both ranges have king-post roof trusses. The northern building may have originally been the smithy and is believed to have had a forge against its southern wall, which survived until about 1979. There are two cast iron vents in the eastern wall. Adjoining the southern side as a timber-built shed with a corrugated iron roof which has a single timber louvered vent along the apex. The southern side has been altered to accommodate a higher sliding door with the loss of the wall plate and part of the roof.

There is no internal detail visible, partly due to the buildings having been stripped for use as a builders' merchants, and partly because some of the modern internal fittings remain.



Plate 5 Canal Maintenance Yard Smithy and Workshops

- 4.10 The most interesting building is the saw shed (Fig. 8; Plate 6), which is two-storeyed, of red brick construction with blue brick sills, and a slate roof. The saw pit itself has been infilled and although it was not possible to gain access to the whole building some information on internal details was forthcoming from records held by the NMR. The first floor is divided into two rooms, originally accessed via two external timber staircases, neither of which survives. The western room had a cast iron range against the dividing wall, and an octagonal window in the western gable, which may have originally held a clock.

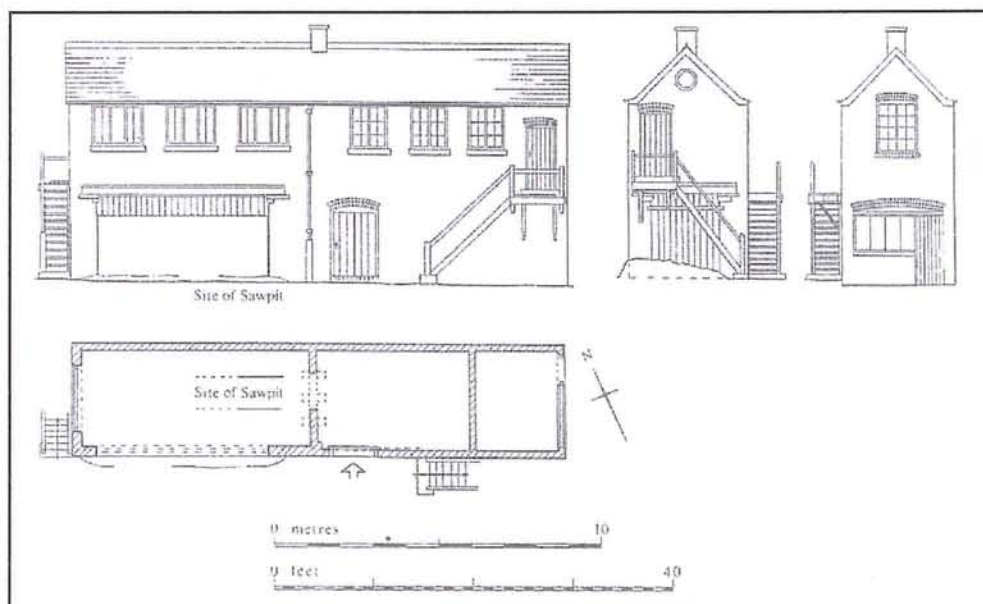


Fig. 8 Plan and elevations of the saw shed (after Hughes 1988, Fig. 99)



Plate 6 Canal Maintenance Yard Saw Shed

- 4.11 To the rear of the workshops is a timber building, presumably a store, with a lean-to along the south side (Plate 7). The building has a timber frame with vertical timber cladding, king-post roof trusses and a slate roof. Access is gained by sliding doors at the west end, which would have been adjacent to a branch of the narrow gauge railway.



Plate 7 Canal Maintenance Yard Store

5 ASSESSMENT AND POTENTIAL

- 5.1 A total of four archaeological sites and features were identified within the proposed development area, together with a number of significant sites within the immediate area. All sites are referred to by their primary record number (PRN) and are tabulated below in Tables 1 and 2.

Classifications

- 5.2 Archaeological sites and landscapes represent a non-renewable resource, and should be avoided wherever this is feasible in order to avoid damage or destruction. All sites can be classified according to a system based on that given in the Department of Environment, Transport and Regions' Design Manual for Roads and Bridges Volume II Section 3 Part 2 (1993). This provides a standard for assessments of this type. Category E (the equivalent of category U in some equivalent classifications) has been introduced to cover archaeological sites and monuments whose existence went unacknowledged in the Design Manual. The categories are defined below.

- *Category A* sites are those believed by CPAT to be of primary significance, either potentially of national importance or already designated by Cadw as scheduled ancient monuments or listed buildings. It is presumed that sites in this category will be preserved and protected *in situ*.
- *Category B* sites are sites of regional importance. These sites are not of sufficient importance to justify scheduling, but are nevertheless important in aiding the understanding and interpretation of the archaeology of the region. Preservation *in situ* is always the preferred option for these sites, but if loss or damage is wholly unavoidable, detailed recording appropriate to the importance of the site must be undertaken.
- *Category C* sites are sites of local importance. These sites are of lesser importance, but are nevertheless useful in the understanding and interpretation of the archaeology of the local area. They may not be of sufficient importance to justify preservation if a development threat is unavoidable, but merit adequate recording in advance of loss or damage, or if portable they should be moved.
- *Category D* sites are either sites or features of minor importance or those which are so badly damaged that too little now remains to justify their inclusion in a higher grade. Rapid recording may be sufficient, but as with Category C sites they could be moved if this is an appropriate strategy.
- *Category E* sites are sites which have been identified, but whose importance cannot be assessed from fieldwork and desk-based study alone. An archaeological evaluation would generally be required to categorise such a site more accurately if the proposal was likely to affect it in any way.

- 5.3 The classification of each site (with the exception of scheduled sites and listed buildings) is based on the collective judgement and expertise of the field staff of CPAT. However, it should be stressed that the significance of a site and hence its classification could readily be enhanced by more detailed work.

Scheduled ancient monuments

- 5.4 On the basis of information provided by Cadw, it is clear that there are no scheduled ancient monuments within the study area.

Listed Buildings

- 5.5 On the basis of information provided by Cadw, there is one listed buildings within area, Dolanog Cottages, which is classified as Grade II. As all listed buildings, regardless of their category, are

considered by the authorities to be of national significance, the fact that that it is only Grade II and not a higher grade is of little import.

- 5.6 There are a significant number of listed buildings within the immediate locality (see Fig. 1), the most notable being Nos 1-4 Clive Place, the Town Wharf (now Powysland Museum), the former canal warehouse off Severn Street, and Welshpool Town Lock, all of which are Grade II.

Conservation designations

- 5.7 The development site lies within the Welshpool Conservation Area. According to the Powys Unitary Development Plan 'any development within or adjacent to the Conservation Area will have to be sympathetically designed to preserve or enhance its character and setting . . . The Montgomery Canal is a candidate Special Area of Conservation (cSAC) and is a designated Site of Special Scientific Interest through Welshpool.'
- 5.8 The site of the former canal maintenance yard is a key feature of the canal landscape in Welshpool and, together with the adjacent locks and canal warehouse/museum, could form a significance focus in the proposals to reopen the waterway. Although some of the buildings within the yard have been lost those that survive have considerable group value, both in the context of the maintenance yard and the wider canal landscape.

Table 1: Sites of Archaeological and Cultural Heritage Interest within the development area

PRN	Name	NGR	Type	Period	Category
30738	Dolanog Cottages	SJ2265207309	House	Post medieval	A
84209	Sergeant's Row	SJ22655 07330	House	Post medieval	E
84210	Canalside Timber Yard	SJ22570724	Saw mill	19 th - century	E
85911	Canal Maintenance Yard	SJ2257907240	Canal works	19 th -century	B

Site descriptions

PRN 30738 Dolanog Cottages

The building is depicted on a late 18th-century estate map and the earliest phase, a single-storey, timber-framed house, is 18th-century in date, or possibly slightly earlier. Later modified with the addition of a first floor and an eastern extension to create two adjoining cottages. Listed Grade II, although now in very poor condition. Considerable potential for surviving constructional and internal detail presently hidden by later wall coverings and vegetation.

PRN 84209 Sergeant's Row

The now demolished row of cottages fronted the southern side of the original line of Severn Street. Evidence from an early 20th-century postcard suggests that the three cottages nearest to the canal were originally timber-framed and are likely to date from the 18th century, or slightly earlier. The remainder of the row is likely to have been brick-built, probably dating from the late 18th century. Interestingly, however, the 1747 map of Welshpool appears to depict only the eastern end of the row which may suggest that these were the earliest buildings, or were possibly replaced at some date. Although there is no surviving trace for any of the buildings, there is significant potential for buried archaeological remains to survive.

PRN 84210 Canalside Timber Yard

A timber yard is depicted on the Tithe Survey of 1840, comprising two buildings leased by David Jones from the Earl of Powis. Although there is no surviving trace of either of the buildings, there is the potential for buried archaeological remains to survive.

PRN 85911 Canal Maintenance Yard

The surviving buildings are the original workshops, saw shed and stores for the Shropshire Union Railways and Canal Company maintenance yard, dating from the second half of the 19th century. Although they are not listed they have considerable group value, and are on British Waterways' list of important heritage sites for the canal.

Table 2: Sites of Archaeological and Cultural Heritage Interest within the immediate area of the proposed development

PRN	Name	NGR	Type	Period	Category
20892	Town Lock Corn Mill	SJ2260007310	Corn mill	19 th -century	D
30732	1 Clive Place	SJ2270707374	House	19 th -century	A
30733	2 Clive Place	SJ2272507361	House	19 th -century	A
30734	3 Clive Place	SJ2274307351	House	19 th -century	A
30735	4 Clive Place	SJ2276007339	House	19 th -century	A
30736	Clive Place 1-4 gates and piers	SJ2272407339	Gateway	19 th -century	A
30745	Town Wharf (Powysland Museum)	SJ2261707354	Warehouse	19 th -century	A
36449	Severn Street Former Canal Warehouse	SJ2269307409	Warehouse	19 th -century	A
36453	Welshpool Town Lock	SJ2259407299	Canal lock	18 th -century	A
72066	1-5 Canal Yard and Salt Warehouse	SJ22590732	Warehouse	19 th -century	B
72069	Town Lock Corn Mill wheel pit	SJ22600731	Wheel pit	19 th -century	B
72070	Welshpool Town Lock weir	SJ22580730	Canal weir	18 th -century	B
85909	Clive Place, canal wharf	SJ2268007407	Canal wharf	19 th -century	B

Site descriptions**PRN 20892 Town Lock Corn Mill**

The Town Lock Corn Mill was sited on the by-pass of the town lock (PRN 36453), taking advantage of the water power from the weir, while not interfering with trade on the canal. The mill is first recorded in the Montgomeryshire Canal Report Books in January 1838 but curiously is not recorded by the 1840 Tithe Survey. Now demolished.

PRNs 30732-6 1-4 Clive Place, including gates and piers

Grade II listed, early 19th-century town houses, with contemporary gates and piers.

PRN 30745 Town Wharf (Powysland Museum)

A brick-built warehouse dating from the second half of the 19th century. The building has two storeys and the main section has a hipped slate roof projecting over the canal. Listed Grade II.

PRN 36449 Severn Street Former Canal Warehouse
Stone-built former canal warehouse. Listed Grade II.

PRN 36453 Welshpool Town Lock

The lock was originally constructed with wooden gates, later replaced with cast iron gates in 1832, which remained in place until the 1970s when they were removed to the Stoke Bruerne Waterways Museum, in Northamptonshire. The lock is listed Grade II.

PRN 72066 1-5 Canal Yard

During the early 19th century a row of buildings was constructed along the western side of the canal wharf to service the commercial needs of the wharf. The earliest buildings are at the southern end of the row, while the numbering runs from north to south: No. 1 was the canal office; No. 2 a salt warehouse and later the Town Lock Keeper's cottage; No. 3 was a house and joiner's shop; No. 4 was the house of the owner of Town Lock Corn Mill; and No. 5 was a house and warehouse owned by the occupiers of the salt warehouse at No. 2 and the granary/warehouse which is now the museum (Hughes 1988, 121).

PRN 72069 Town Lock Corn Mill wheel pit

A wheel pit alongside the Town Lock is all that remains of the former corn mill.

PRN 72070 Welshpool Town Lock weir

A weir constructed in blue engineering brick and located south of the Town Lock on the upper pound with an overflow and feeder for the adjacent corn mill wheel pit.

PRN 85909 Clive Place, canal wharf

A 19th-century canal wharf to the north of Severn Street.

6 CONCLUSION

- 6.1 The assessment has demonstrated that there are a number of significant archaeological sites recorded both within and around the proposed development site, the majority of which are in some way related to the waterway now known as the Montgomery Canal. The development site also lies entirely within the Welshpool Conservation Area.
- 6.2 The earliest surviving site is Dolanog Cottages, which was originally a timber-framed house dating to the 18th century, or possibly slightly earlier, and is a Grade II listed building. A row of cottages formerly stood nearby, known as Sergeant's Row, the earlier elements of which are likely to be broadly contemporary with Dolanog House. Although there is no surviving trace for the row there is significant potential for buried archaeological remains to survive.
- 6.3 The part of the study area formerly occupied by Travis Perkins was a timber yard during the early 19th century, which was later replaced by the canal maintenance yard. All of the surviving buildings on this part of the site were constructed as part of the canal works, and include the saw shed, workshops and a store. The complex was an integral part of canal landscape during the 19th and early 20th centuries and although the buildings are not listed they have considerable group value, both in the context of the maintenance yard and in association with the canal itself and other surviving canal buildings. The buildings of the maintenance yard are typical of the general architecture of the canal. From the information provided by the client it appears that development proposals will lead to the loss of some of the structures and it may therefore be appropriate to consider the possibility of relocating some buildings to another site, in conjunction with British Waterways, as part of the on-going regeneration of the waterway. In addition to the surviving buildings there is also potential for buried remains to survive belonging to the associated narrow gauge railway, as well as the earlier timber yard.

7 ACKNOWLEDGEMENTS

- 7.1 The writer would like to thank the following for their assistance and co-operation: Wendy Owen, CPAT; the staff of the National Monuments Record, RCAHMW in Aberystwyth; the staff of the National Library of Wales in Aberystwyth; Mr Stephen Lees, British Waterways.

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Post-1780 Powis Castle estate map, NLW Powis Castle M17-18.

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1902 Ordnance Survey 1:2,500 2nd edition map of Welshpool.

1983 Soil Survey of England and Wales map (Sheet 2 - Wales) and Legend (1:250,000 scale).

1994 British Geological Survey map of Wales (Solid edition at 1:250,000 scale).

APPENDIX 1

SPECIFICATION

1 Introduction

- 1.1 The Field Services Section of the Clwyd-Powys Archaeological Trust has been invited to submit a tender for an archaeological evaluation in connection with proposals for a new development on land at the former Travis Perkins builders merchants yard and around Dolanog Cottages, off Severn Street, in Welshpool. The Curatorial Section of the Clwyd-Powys Archaeological Trust, in their capacity as archaeological advisors to the local authority, have determined that an archaeological evaluation is required to identify the potential impact on the archaeological resource. Accordingly, a brief has been prepared which details the works required (CPAT EVB 670).
- 1.2 The proposed development is located on the east side of the Montgomeryshire Canal and south of Severn Street in Welshpool, centred at SJ 22580722. Information retained within the Historic Environment Record indicates that during the later 19th century the site was occupied by a canal maintenance yard which included a narrow gauge railway and a number of buildings, some of which are still standing.

2 Objectives

- 2.1 The objectives of the evaluation are:
 - 2.1.1 to reveal by means of a desktop study, rapid building survey and field evaluation, the nature, condition, significance and, where possible, the chronology of the cultural heritage within the area of the proposed development in so far as these aims are possible;
 - 2.1.2 to record any archaeological sites identified during the evaluation;
 - 2.1.3 to prepare a report outlining the results of the evaluation.

3 Methods

- 3.1 Stage 1 of the evaluation will involve the examination of all the readily available primary and secondary documentary, cartographic, pictorial, and photographic sources at the following repositories: the regional Historic Environment Record, Clwyd-Powys Archaeological Trust, Welshpool; the National Library of Wales, Aberystwyth; and the National Monuments Record, Aberystwyth.
- 3.2 All cartographic sources consulted will be included within the desktop section of the report, together with transcriptions of relevant documents and copies of plans, maps and photographs containing relevant information.
- 3.3 Stage 2 will consist of three trenches totalling up to 55m², the locations and dimensions of which will be agreed with the curator in advance but will be positioned to investigate potential remains associated with a turntable and railway, and a row of cottages and outbuildings known as Sergeant's Row.
- 3.4 All excavations will be undertaken using a machine excavator with a toothless bucket to remove modern overburden down to the level of the first recognisable archaeological horizon. Thereafter, all

excavation will be conducted by hand unless otherwise agreed with the Curator in advance. The evaluation will be entirely non-destructive and designed to determine the depth at which archaeologically sensitive deposits survive, together with their nature condition and significance. The depth of natural deposits will be determined to assess the extent of any stratified deposits which may be encountered.

- 3.5 It has been assumed that the area in question has sufficient access for a JCB or other mechanical excavator. Excavated material will be temporarily stored adjacent to the trench, which will be reinstated with this material upon completion. The trenches will be surrounded by 2m security fencing during the excavation if required but no provision has been made for stripping or relaying any surfaced areas or reseeding.
- 3.6 Contexts will be recorded on individual record forms and be drawn and photographed as appropriate. All photography will be in 35mm format black and white print and colour slide. All features will be located as accurately as possible with respect to buildings and boundaries identified on modern Ordnance Survey maps and levels will be related to Ordnance Datum where possible, with the use of total station surveying.
- 3.7 All artefacts will be related to their contexts from which they were derived and treated in a manner appropriate to their composition and will be processed by trained CPAT staff. Provision has been included for sampling deposits for dating, environmental and technological evidence as appropriate.
- 3.8 Stage 3 will consist of a rapid building survey, comprising a basic descriptive record and high resolution digital photographs of all buildings, including interior and exterior views, together with views of any original fixtures and fittings.
- 3.9 Following the on-site work an illustrated and bound report will be prepared in A4 format, containing conventional sections on: Site location, Topography and Geology; Archaeological Background; Evaluation; Conclusions and References, together with appropriate appendices on archives and finds.
- 3.10 The site archive will be prepared to specifications laid out in Appendix 3 in the Management of Archaeological Projects (English Heritage, 1991), to be deposited with the Regional Sites and Monuments Record (SMR). All artefacts will, subject to the permission of the owner, be deposited with Powysland Museum, Welshpool.

4 Resources and Programming

- 4.1 The assessment will be undertaken by a team of two to three skilled archaeologists under the direct supervision of Mr R J Silvester, a senior member of CPAT's staff who is also a member of the Institute of Field Archaeologists. CPAT is also an Institute of Field Archaeologist Registered Organisation.
- 4.2 All report preparation will be completed by or with the assistance of the same field archaeologist(s) who conducted the assessment.
- 4.3 It is anticipated that the evaluation will be completed within 5 days with a team of three archaeologists, and the report will be completed within 5 days following the on-site work. As required in section 7.1 of the curatorial brief a draft report will be presented to the curator prior to the submission of the final report. At present CPAT would be in a position to commence the evaluation within three weeks of receiving written instruction from the client.
- 4.4 The following contingency sums have been allowed at the request of the Curator.

Curatorial monitoring	£50 per visit
Publication	<i>Archaeology in Wales</i> at no additional charge
- 4.5 Requirements relating to Health and Safety regulations will be adhered to by CPAT and its staff.

- 4.6 CPAT is covered by appropriate Public and Employer's Liability insurance, as well as Professional Indemnity insurance.

N W Jones
17 July 2007